

city of charlottetown
500 lot area
development standards &
design guidelines

The**Planning**Partnership
December 2011



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500 Lot Area

The 500 Lot Area encompasses the City of Charlottetown's historical, cultural, civic and symbolic core. It was here that the Charlottetown Conference was held in 1864 that led to Canada's confederation in 1867, and where the primary capital functions of the Province take place. Thousands of tourists are destined for this area every year, arriving either by land, air or water. The 500 Lot Area continues to function as a primary shopping, education, entertainment, recreational, service and employment hub for residents throughout the region. At the same time, most of the area is comprised of charming, established historic residential neighbourhoods that over 5,000 people call home.

Consistent with the experience of many other downtown areas in comparable North American cities, the 500 Lot Area is on the cusp of a renaissance. After decades of decline due to suburbanization, the residential population is on the rise, new businesses are being established, heritage buildings are being restored and the waterfront is on the verge of exciting transformation. With this renewed interest in the 500 Lot Area comes development pressures that, if not carefully considered, may undermine the very qualities and characteristics that define this area and that lend to its distinct 'sense of place'. On the other hand, with appropriate guidance and care, new development presents an opportunity to restore heritage resources, fill the 'gaps' in the streetscapes and on the waterfront, and repopulate the area back to historic levels.

Purpose

The current regulatory framework may have served the 500 Lot Area well in the past, however it is increasing becoming antiquated in light of recent approvals and proposals that have challenged and far exceeded the in-force standards and requirements. Furthermore, the zoning by-law is too blunt a tool for guiding infill development in such a context, while the design principles applied in the Heritage Area are often subject to differing interpretations. Consequently, on one hand the residential and heritage communities feel that the area's historic character is under threat, while on the other hand, the development community feel frustrated by an unpredictable approval process. All are seeking greater clarity with respect to standards and guidelines for development, as well as a more consistent process for review and approvals.

The primary purpose of these *Development Standards & Design Guidelines (Standards & Guidelines)* is to direct and shape the ongoing development of the 500 Lot Area in a balanced manner and according to good urban design principles. In particular, protecting and reinforcing the area's distinct history and built characteristics, while enabling investment and revitalization opportunities through appropriate development. The intent of this document is to provide the City with a sound and rational framework for regulating development and for consistently assessing proposals.

Background & Process

In August 2010, The Planning Partnership was retained by the City of Charlottetown to prepare development standards for the 500 Lot Area. A key initial task was to review and build on the tremendous body of work already undertaken. Specifically, these *Standards & Guidelines* are especially informed by two recent comprehensive studies prepared for the City of Charlottetown:

- *Downtown Charlottetown 500 Lot Strategy* (Ekistics, June 2006)
- *Development Action Plan for Charlottetown's 500 Lot Area* (Dalhousie University, April 2010)

These studies involved significant consultation; provided detailed and extensive background information; and, outlined recommended strategies and actions for addressing both heritage and development in the 500 Lot Area. Where relevant and appropriate, some of these recommendations, or variations of them, have been incorporated into this document.

The foundation for these *Standards & Guidelines* was established through an intensive, weeklong 'workshop' based in City Hall between September 20th and 24th, 2010. Over the course of five days, the Consultant Team undertook a number of key tasks that included:

- Meetings with City Staff and Councillors
- Meetings with stakeholders representing a broad spectrum of interests, including residents, businesses, developers, heritage, and architects (the stakeholders are identified Appendix A)

- Touring and detailed surveying and analysis of the 500 Lot Area
- Preparation of an approach, principles, frameworks and draft standards and guidelines
- Facilitating a public meeting at the Delta Hotel, to present for feedback the initial approach for what would become the basis for the *Standards & Guidelines*

The draft *Standards & Guidelines* were subsequently presented to Council and at Public Meeting in late September 2011. This provided an opportunity for further feedback and refinement for finalizing the document.

Although the *Standards & Guidelines* are rooted in enduring urban design principles and best practices, the outcome of the workshop and consultation with City staff, stakeholders and the public was fundamental to tailoring the document with respect to its organization, emphasis and the recommended implementation strategies.

Document Organization

The Standards & Guidelines document is organized into the following components:

Guiding Principles – Section 2.0 outlines the guiding principles and accompanying rationale that underpin the approach to the Standards & Guidelines.

Urban Design Framework – Section 3.0 provides the overarching physical framework for which the Standards & Guidelines apply, including Urban Character Areas, Streetscapes, and Civic Elements.

Development Standards – Section 4.0 provides measurable form-based standards for development organized according to the Urban Character Areas, and include guidance for elements such as uses, heights, setbacks and parking.

Design Guidelines – Section 5.0 provides mostly qualitative design guidance for development to support and work in conjunction with the Development Standards.

Implementation Strategies – Section 6.0 provides recommendations for the implementation of the Standards & Guidelines, as well as other recommended actions and initiatives relating to development that may benefit the 500 Lot Area.

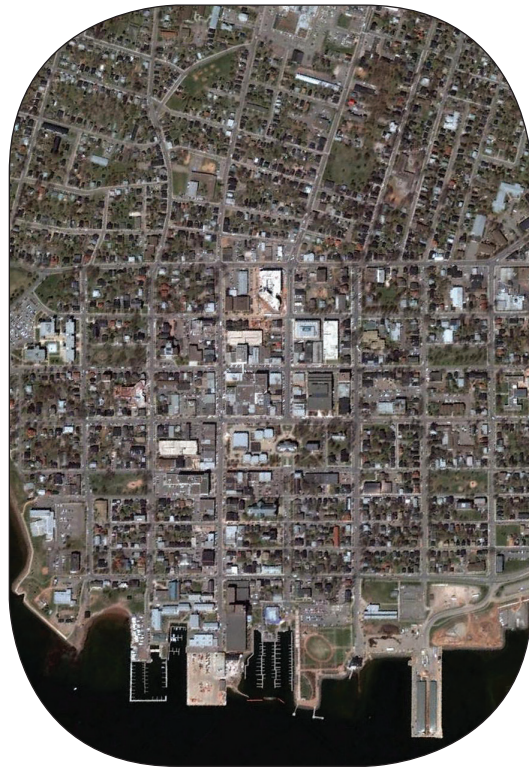


2.0 guiding principles

The following ten Guiding Principles underpin and inform the approach to the *Standards & Guidelines*.

1

The 500 Lot Area is A Special Place Warranting Special Rules & Privileges



Rationale

The 500 Lot Area is ‘special’ not only for its history, function and built character, but also because of its tremendous complexity and challenges relative to other parts of the city. Servicing, maintaining, investing and developing in historic urban areas are often more complicated, expensive and even risky undertakings. At the same time, this is the heart and soul of the city, where iconic architectural landmarks define its image and where the impression on thousands of visitors is ultimately shaped.

More than any other district or neighbourhood in the city, if not the province, the 500 Lot Area is a broadly treasured and shared amenity. Furthermore, because the health, vibrancy and prosperity of a downtown are equated with the health, vibrancy and prosperity of its host city, all citizens of Charlottetown benefit from its success. Accordingly, this area warrants a special status within the city so as to respond to both its true potential and to the inherent challenges in realizing that potential. This translates into initiatives such as greater public investments in the area; alternative standards and regulations; incentive programs and mechanisms; and, creative public-private partnerships to better deliver on development objectives and design excellence.

2

Reinforce the Existing Urban Structure



Rationale

Likely in large part due to minimal development pressures over the last few decades, the 500 Lot Area’s traditional urban structure remains mostly intact. Unlike other larger centres in Atlantic Canada where inappropriate development decisions have undermined the continuity and coherence of districts, the 500 Lot Area continues to retain visibly distinct built character areas that are also hierarchal and transitional.

Coined by New Urbanists as an urban ‘transect’, this transitional order and organization is generally characterized by a mixed-use dense core at one end of the continuum, stepping down to low-rise residential areas at the other end. New developments should reinforce this hierarchy by ensuring the uses, scale and building typologies are consistent with the location of the site within the urban character continuum. The urban structure for the 500 Lot Area is further defined in the *Urban Character Areas* framework, which is the basis of the *Development Standards*.

3

Reinforce & Extend the Historic Street & Block Pattern



Rationale

The 500 Lot Area is well recognized for its walkability by local residents and especially by visitors. This quality is due in large part to the inviting character of the streetscapes and the fine-grained, grid pattern of streets and blocks. This pattern results in a highly interconnected network of streets and small blocks that make the entire area very accessible and easy to navigate on foot.

The street and block pattern is a fundamental defining aspect of the 500 Lot Area's historic character. Altering this pattern through street closures such as Cumberland Street at Holland College should be discouraged and only considered where it results in an improvement of the public realm, such as a public space, while still retaining unfettered pedestrian access and views. The existing street grid should extend into large development sites and with blocks similar in scale to adjacent areas.

4

Protect, Restore, Respect & Leverage All Heritage Resources



Rationale

The most prevalent asset of the 500 Lot Area is the intact historic fabric comprised of an extraordinary concentration of significant heritage buildings, landmarks and streetscapes that span over 200 years. Nearly all architectural styles that emerged in this region over this period are represented, up to and including modernism. These resources play a prominent role in defining the distinct 'sense of place' of the Lot 500 Area, which also happens to be its greatest appeal and draw for visitors and tourists. This would be consistent with the fact that heritage as an attraction in its own right is the leading factor in the revitalization of North America's most successful downtowns in small to mid-sized cities.

Beginning with Heritage Provisions introduced in the 1979 By-law, Charlottetown is a leader in heritage protection, including supportive regulations, guidelines, development review, as well as grants to assist landowners with restoration. However, many resources remain unprotected and will increasingly come under threat with growing development pressures in the area. Heritage should be recognized as the life-blood of the area's civic, cultural and economic well-being. All resources should be protected and restored. New developments should be complementary, maintain its visual prominence, and contribute to its vitality.

At the same time, heritage protection should not become an impediment to the area's growth, development and modernization. The 500 Lot Area is not a museum, rather, it is a living and ever-evolving community. Authentic and functioning downtowns are far more appealing for tourists as they offer variety, vibrancy, culture and modern conveniences, which in turn can result in repeat visits. In this context, development could be appreciated as a means for protecting and investing in heritage resources.

5

Protect & Strengthen the Character of the Residential Neighbourhoods



Rationale

The scale and magnitude of development will not occur uniformly across the 500 Lot Area. Of particular importance to any downtown area is the health and stability of its surrounding residential neighbourhoods. These neighbourhoods have treasured historic value, lend to the area's charm, and ensure a broad mix of housing and demographics within close proximity of the downtown and its businesses. However, with the area's revitalization, these neighbourhoods will be subject to greater development pressures. A growing residential market for living in the area combined with increasing land values can result in infill or redevelopment that is out of scale and character with the neighbourhood and can undermine its stability.

Key to building support for significant development in the downtown is assuring that neighbourhoods will be protected. Their stability should be reinforced with certainty about the scale and nature of change that can be anticipated. Specifically, these *Standards & Guidelines* have been designed to ensure that change will be modest and consistent with defining character of these areas with respect to uses, scale, building types, and setbacks.

6

Provide Transitions Between Areas of Differing Intensities & Scales



Rationale

An important characteristic of the existing urban structure of the 500 Lot Area, is the gradual transitions between differing urban character areas. This is common to historic downtown areas and is more an outcome of many years of urban evolution than of deliberate planning. However, contemporary planning practice recognizes the merits of providing for transitions to effectively mitigate between areas of differing land uses and densities. Providing appropriate transitions are especially important where intensification is occurring near or adjacent to established neighbourhoods. For example, stepping down the scale and height of a development and providing for compatible uses can mitigate the visual and physical impact on adjacent homes.

The *Urban Character Areas* identified in these *Standards & Guidelines* provides a rational framework that transitions uses and scales for development across the 500 Lot Area. Furthermore, the *Development Standards* within each of these areas provides guidance for appropriate transitions within a given development where it is abutting a house residence or adjacent neighbourhood.

7

The First 3-storeys Fronting the Street Matter the Most



Rationale

The 500 Lot Area is experienced primarily through its streetscapes and open spaces, the quality of which is shaped and defined by the buildings that frame them. As with most historic centres, this is a pedestrian-oriented environment where most activities are accomplished on foot. Whether a building is 2 or 8-storeys, it is the first levels and how they interface with the street that makes the greatest impact on the quality of that walking experience.

The *Standards & Guidelines* seek to ensure that new developments in the 500 Lot Area are designed with careful consideration for how the first 3-storeys interface with the street and provide visual interest. Buildings should properly address the sidewalk with frequent entries and windows, and particular attention should be paid to the quality of materials and architectural detailing. Shop fronts should animate the street with prominent heights, high levels of transparency, and narrow human-scaled widths. Higher levels should step-back above the 3rd storey to reinforce the area's prevailing low-rise street wall.

Blank or inactive grade levels, as well as pedestrian bridges or tunnels over or under streets should be prohibited to protect the pedestrian environment and to ensure its maximum animation.

8

Strengthen Visual & Physical Orientation & Connectivity



Rationale

The 500 Lot Area has a rich tradition of urban design that celebrates civic life through splendid architectural landmarks that are strategically located and designed to enhance their visual prominence. The design and placement of these buildings mark important locations and/or provide recognizable visual cues of their function. As distinctive landmarks, they also reinforce a sense of place and identity, enhance civic pride and serve as orienting devices. For visitors, landmarks can enhance connectivity by providing visual interest, which attracts and stimulate pedestrian movement.

The *Standards & Guidelines* seek to build in this urban design tradition by identifying visually prominent sites at key intersections, gateways and view corridors. New developments, public art or public buildings should reinforce these locations in their placement, massing and architectural design.

9

Larger & Taller Buildings Have the Greatest Civic Responsibilities



Rationale

The 500 Lot Area has a long history of large and tall buildings. Historic buildings, such as St. Dunstan’s Basilica or the Rodd Charlottetown Hotel, exhibit architectural grandeur that corresponds to their scale and civic importance. The way they are situated on their sites, the expressed massing, material quality, and design detail, all work in concert to enhance their stature while positively contributing to their context. By contrast, more contemporary large-scaled developments such as the Delta Prince Edward, exhibit far lesser care for their context or design quality. Consequently, by virtue of their scale, they sit prominently and distractingly in the streetscape, constantly reinforcing the perception of large-scale being synonymous with bad design.

The *Standard & Guidelines* recognize that large-scaled buildings are, and will continue to be, part of the urban fabric of the 500 Lot Area. However, these developments should be directed only to where they already exist and can be accommodated, and subject to stringent conditions and performance standards. Appropriate setbacks and massing are especially important to ensuring that these buildings do not overwhelm and adversely impact streetscapes and adjacent properties with respect to matters such as sky view, wind, and shadows. Given their visual prominence, these buildings should be held to the highest design standards, exhibiting landmark architectural qualities. Furthermore, these development rights ought to be privileged through a bonus afforded only in exchange for public benefits such as heritage protection, community amenities, or public realm improvements.

10

Design & Construct Buildings so that they Become Future Heritage



Rationale

The 500 Lot Area includes an extraordinary assembly of architecturally significant heritage buildings. Some include notable contemporary landmarks such as the Confederation Centre of the Arts and the Dominion Building, but most noteworthy structures date to pre-war eras. That these historic buildings have endured this span of time and continue to have broad market appeal is a testament to the quality of their architectural design, material longevity, and sturdy construction. By contrast, the majority of ‘modern’ buildings constructed in the post-war era would not measure up by any standard. Consequently, new poorly designed and constructed buildings are often wrongly associated with contemporary architecture.

New developments in the 500 Lot Area should reflect architectural design that is of its time. This approach is consistent with best practices in heritage conservation, which strongly discourage historic mimicry and pastiche in the design of new buildings, as it undermines the value and authenticity of the real thing. Rather, new buildings should complement this context through contemporary architecture that exhibits a quality that is equivalent in design excellence, detailing, materiality and construction. To do so, higher design standards should be incentivised and expected, made integral to the review process, and acknowledged or rewarded when it is achieved.



3.0 urban design framework

The Urban Design Framework provides the overarching physical framework for the *Standards & Guidelines*. This Framework is necessary for understanding the varying patterns and elements that define the 500 Lot Area, as well as the unrealized opportunities. This informs and organizes the subsequent *Development Standards* and *Design Guidelines* sections.

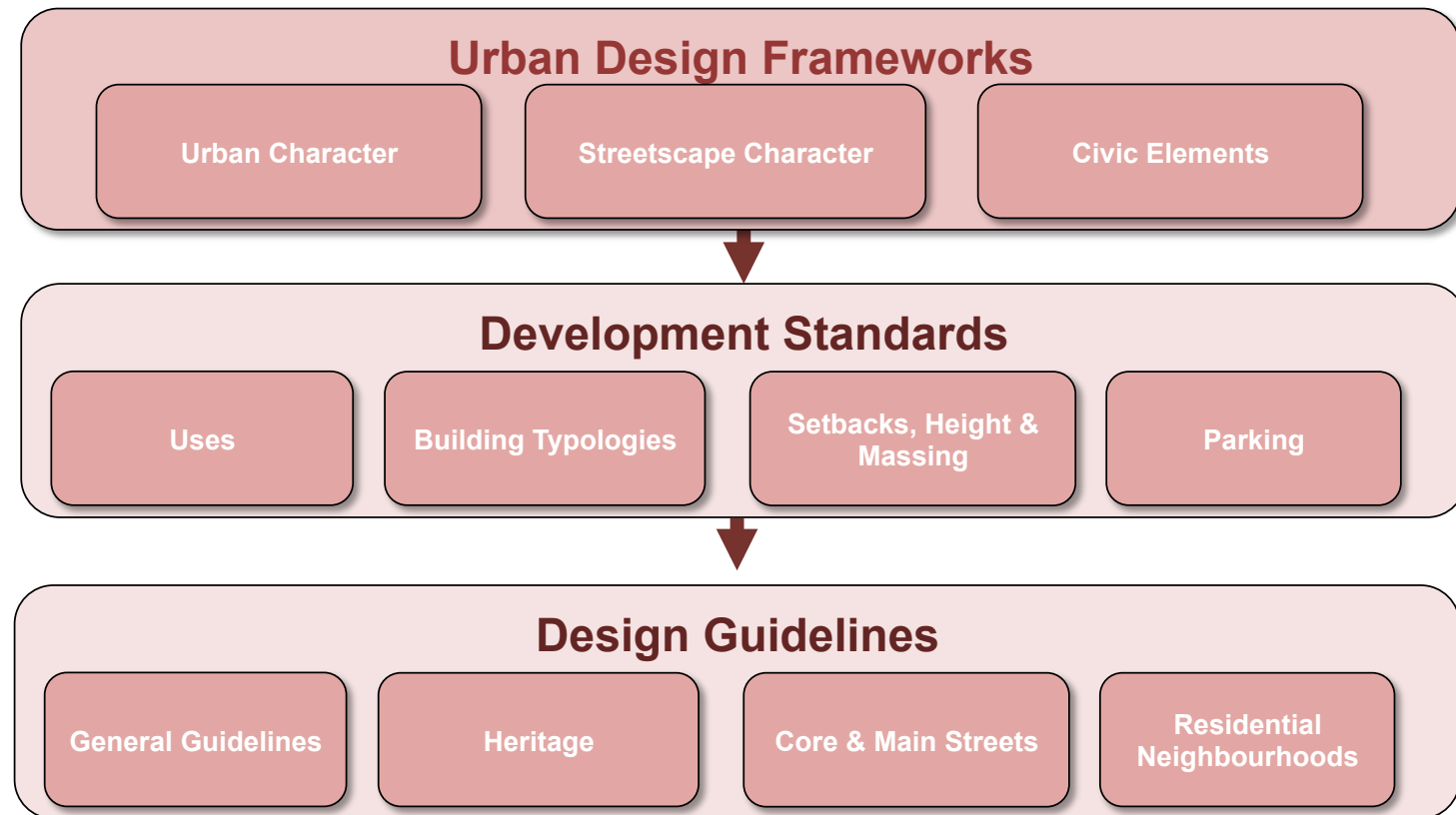
The Urban Design Framework includes the following three interrelated framework plans:

Urban Character Areas – identifies the different character areas as defined by the prevailing uses, building typologies and development characteristics such as building scale and placement, as well as parking arrangements.

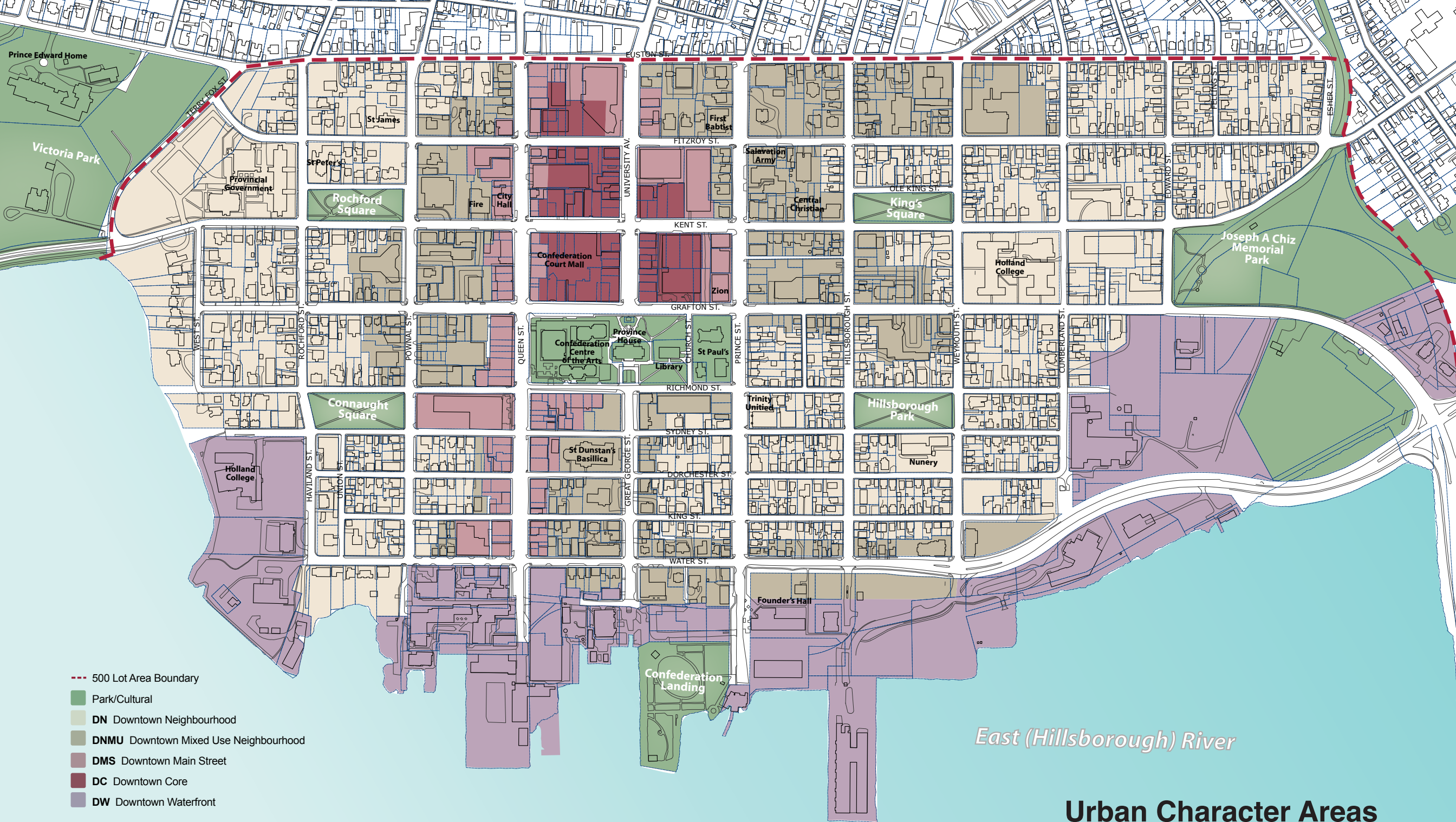
Streetscapes – identifies the key different streetscape conditions in the 500 Lot Area as defined primarily by the width of the right-of-ways, and the manner in which grade-level uses interface with the sidewalk.

Civic Elements – identifies the various components that lend to the quality and experience of the public realm as defined by elements such as heritage resources, landmarks, and gateways.

Together, these framework plans define the urban design quality and potential of the 500 Lot Area, and help to guide the appropriate configurations, scale and design of developments.



The Urban Design Framework organizes and informs the Development Standards and Design Guidelines



- - - 500 Lot Area Boundary
- Park/Cultural
- DN Downtown Neighbourhood
- DNMU Downtown Mixed Use Neighbourhood
- DMS Downtown Main Street
- DC Downtown Core
- DW Downtown Waterfront

East (Hillsborough) River

Urban Character Areas

3.0 urban design framework: urban character areas

3.1 Urban Character Areas

Development and change in the 500 Lot area will not occur uniformly. The Urban Character Areas Framework provides a rational hierarchy for guiding appropriate uses and forms of development that is consistent with the transitional built character of the district. The prevailing existing and emerging land uses, scale of buildings, and building typologies primarily define the different character areas.

This Framework reinforces a legible urban structure and ensures that there is a transition in the scale and intensity of uses and built form between areas of differing character. In particular, it informs the *Development Standards* outlined in Section 4.0 with respect to appropriate uses, setbacks, heights, massing, parking arrangements and building typologies for new developments.

Key components of the Urban Character Areas Framework include:



Downtown Neighbourhood – are areas that have a prevailing residential use and character and are generally focused on the Rochford, Connaught, Hillsborough and King’s Squares. These areas are defined by a variety of residential building typologies of differing architectural styles, but predominantly in 2-3 storey house forms such as detached, semi-detached and row housing. These areas also include some apartment forms and institutional buildings. Front-yard setbacks vary depending on the street and on-site parking is at the side or rear of the properties. Changed in these areas should be modest and primarily in the form of small-scale residential renovations, infill and additions that is consistent with the surrounding built character.



Downtown Mixed-Use Neighbourhood – are areas that are very similar to Downtown Neighbourhoods in built character except for a greater mix of uses, often in contained within house form building typologies. Apartment and office buildings, as well as parking structures exist and should continue to be permitted with careful design considerations. These areas generally provide a transition between the predominantly residential uses and the more intense and larger scaled buildings associated with the Downtown Main Street and Core areas. Although the uses in these areas may be more transitional in nature, the prevailing residential character of the built form should remain in tact.



Downtown Main Street – are the mixed-use corridors generally associated with Queen Street and segments of other streets around the Core. These areas are predominately defined by 2-4 storey continuous buildings placed close the street edge and consisting of narrow storefronts at the street level, with office and residential uses on the upper levels. There are also a number of existing or approved buildings that range between 5-6 storeys. Infill or redevelopment in these areas should reinforce these characteristics and with sensitivity where heritage is impacted. Where appropriate and under certain conditions, taller buildings or additions up to 6-storeys may be considered through a bonus in exchange for heritage protection or other public benefits. On-site parking is to be located to the rear or below-grade.

Downtown Core – defines the area with the greatest intensity of commercial and office use and the tallest buildings in the Downtown. This area is generally comprised of continuous retail uses at-grade with heights that range between 2-10 storeys. The taller buildings include office and hotel uses. Most structures are relatively contemporary. Significant infill or redevelopment can be anticipated in this area and of the largest scale in the 500 Lot area. Where appropriate and under certain conditions, taller buildings or additions up to 8-storeys may be considered through a bonus in exchange for heritage protection or other public benefits. Up to 10-storey would only be permitted on sites that can provide significant setbacks from the street. No building is to be taller than the spires of the St. Dunstan’s Basilica. On-site parking is to be located to the rear or below-grade.



Downtown Waterfront – defines the large scale and evolving lands associated with the central waterfront area. These areas currently contain a variety of uses and building types. Given the scale of the lands and the lack of a well-defined organizing structure, these areas require comprehensive planning when considered for development. This is to ensure a coordinated and cohesive outcome that includes a fine-grained network of streets and blocks, open spaces and other amenities. In general, the uses should be mixed, heights should step down to the waterfront and grade-level uses should animate the water’s edge in all seasons.



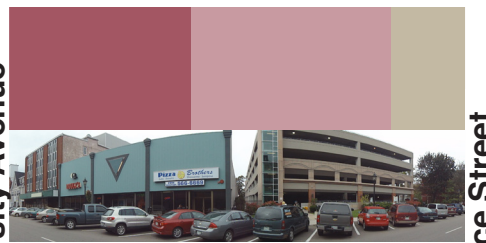
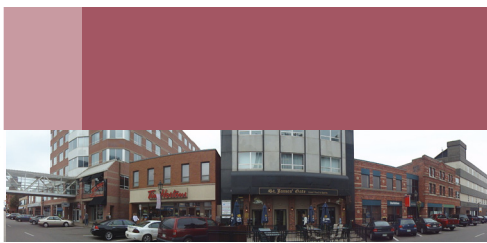
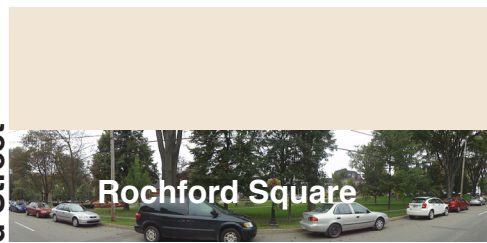
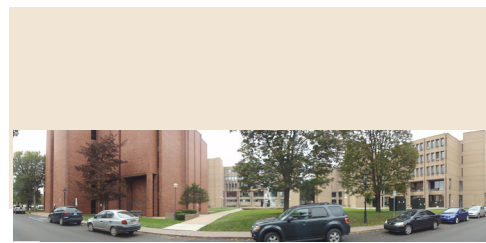
Downtown Neighbourhood

Downtown Mixed-Use Neighbourhood

Downtown Main Street

Downtown Core

Downtown Main Street



West Street

Rochford Street

Rochford Square

Pownal Street

Kent Street

Queen Street

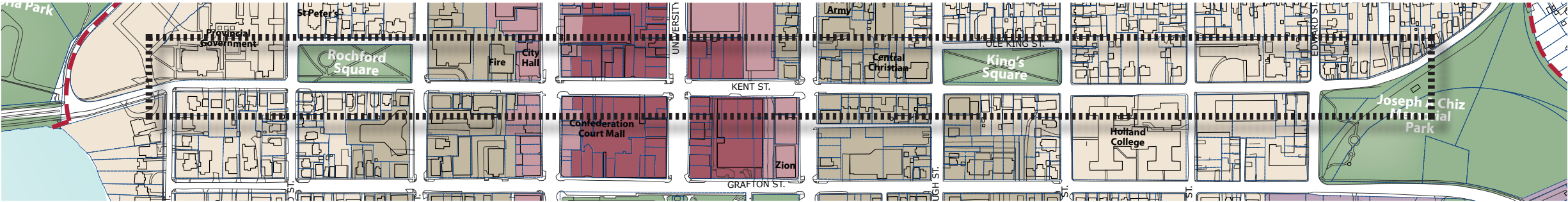
University Avenue

Prince Street

3.0 urban design framework: urban character areas

Urban Character Areas Visualization: Kent Street

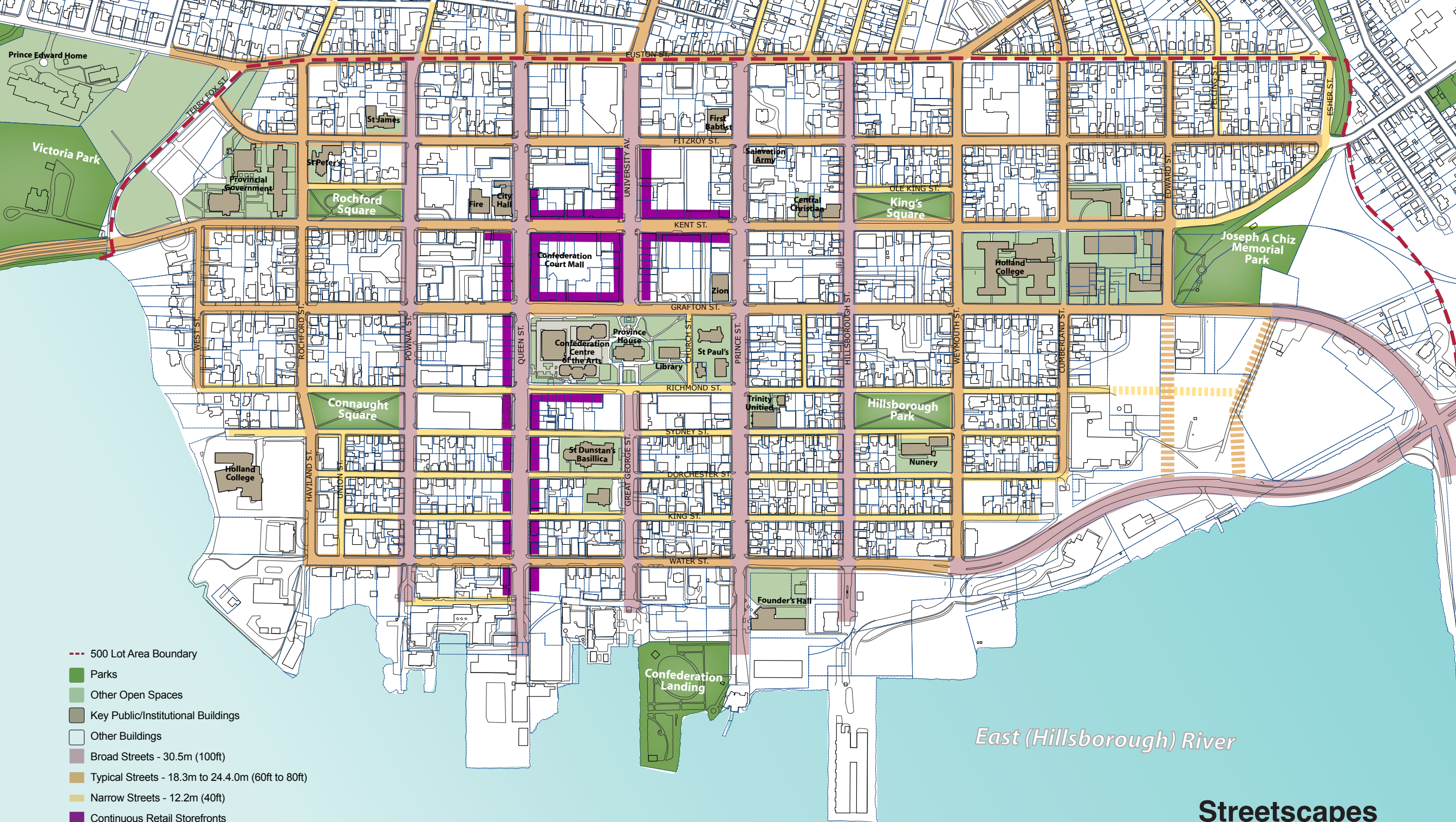
To visually demonstrate the transitional and distinct built qualities of the Urban Character Areas, a photographic inventory of both sides of Kent Street is provided. The extent of the visualization area is identified in the Urban Character Area plan below.



**Downtown
Mixed-Use
Neighbourhood**

Downtown Neighbourhood





- - - 500 Lot Area Boundary
- Parks
- Other Open Spaces
- Key Public/Institutional Buildings
- Other Buildings
- Broad Streets - 30.5m (100ft)
- Typical Streets - 18.3m to 24.4.0m (60ft to 80ft)
- Narrow Streets - 12.2m (40ft)
- Continuous Retail Storefronts

East (Hillsborough) River

Streetscapes

3.0 urban design framework: streetscapes

3.2 Streetscapes

The streets in the 500 Lot Area are the most significant and enduring historical features, remaining largely intact since the founding of Charlottetown 240 years ago. The distinctive hierarchy comprised of both grand and broad streets as well as charming and narrow ones, create the extraordinary diversity of experiences from one block to the next and is fundamental to the area's distinct sense of place. The grid of streets is also finely interconnected creating small blocks that lend to the area's walkability.

The varying street right-of-ways greatly define the character of the streetscapes as it impacts the sense of scale and how buildings interface with the street with respect to uses, front-yard setbacks, and building elements such as porches. Infill or redevelopment will need to be responsive to the character of the streetscape being addresses.

Broad Streets – have the widest street right-of-ways in the 500 Lot Area. In residential character areas, houses tend to be larger with porches and generous landscaped front yards.



Typical Streets – have narrower right-of-ways than Broad Streets and are more typical of traditional residential streets, accommodating a variety of front-yard conditions and often depending on the architectural style of the buildings.

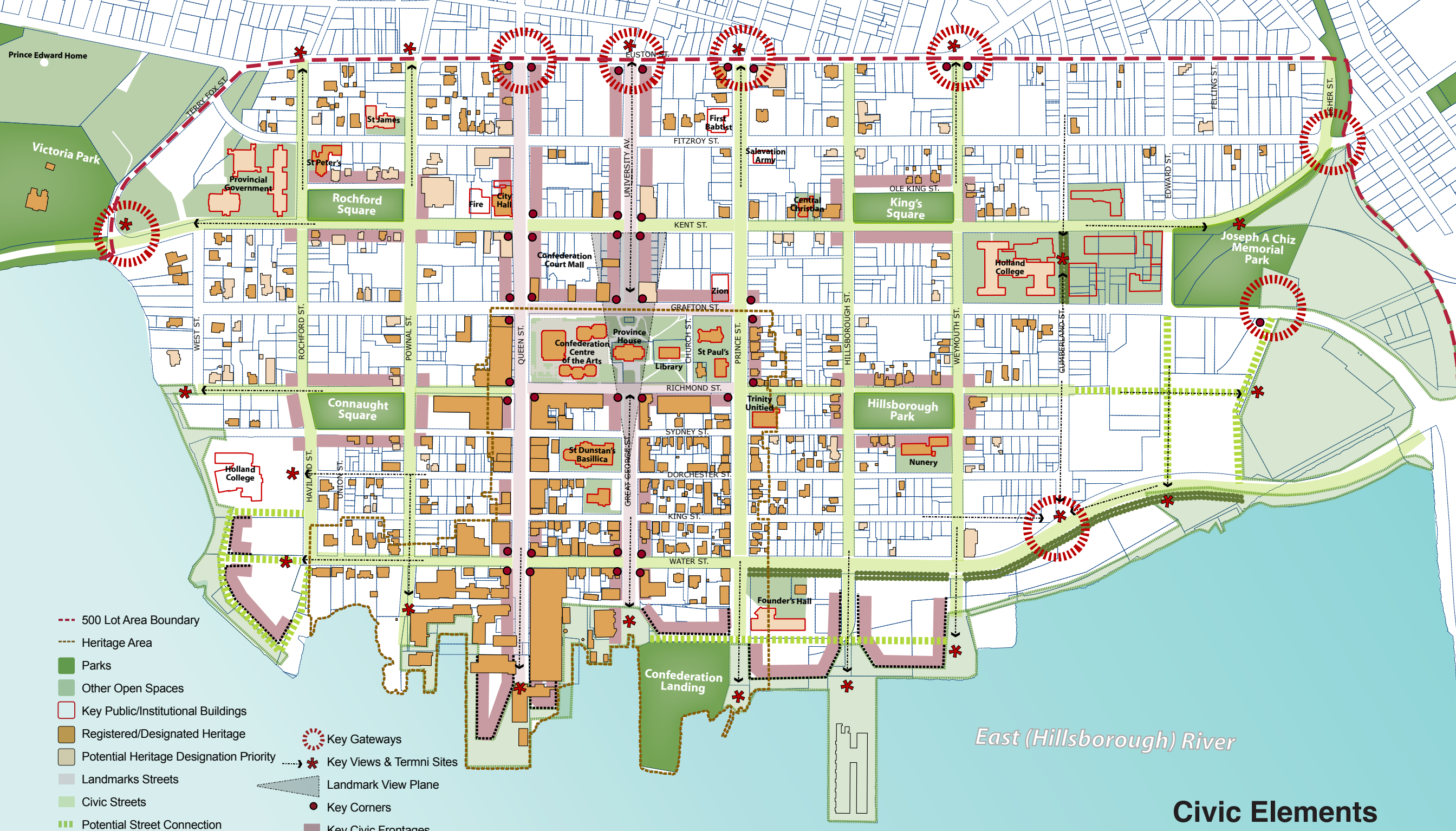


Mews- are exceptionally narrow street right-of-ways often characterized by buildings placed very close to the street edge, and usually affording space for no more than a modest stoop.



Continuous Retail Storefronts – identify the street frontages that have continuous and generally narrow storefronts. Along these streets, buildings are consistently placed close to or at the street edge and with frequent entrances and generous display windows that animate the sidewalk.





- 500 Lot Area Boundary
- Heritage Area
- Parks
- Other Open Spaces
- Key Public/Institutional Buildings
- Registered/Designated Heritage
- Potential Heritage Designation Priority
- Landmarks Streets
- Civic Streets
- Potential Street Connection

- ⊙ Key Gateways
- ⋆ Key Views & Termini Sites
- ▲ Landmark View Plane
- Key Corners
- Key Civic Frontages

East (Hillsborough) River

Civic Elements

3.3 Civic Elements

The Civic Elements Framework defines the existing and potential aspects of the 500 Lot Area that reinforce a distinct ‘sense of place’ and that enhance the experience one has in moving through the district.

Together these elements lend to the celebration of history, provide a sense of entry, help to orient visitors, highlight important landmarks and streets, and strengthen visual and physical connections between the downtown, neighbourhoods, open spaces and waterfront. New developments should be consistent with and reinforce these civic elements.

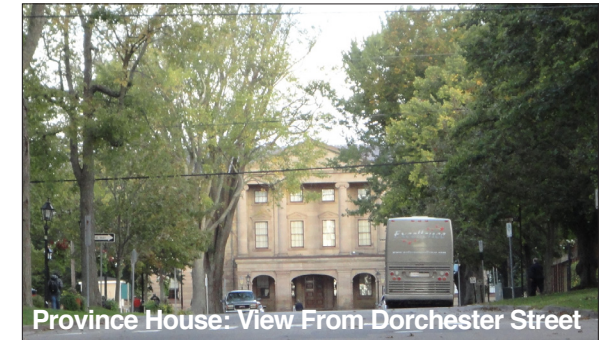
The key components of the Civic Elements Framework include:



Registered/Designated Heritage – buildings on municipally, provincially and/or federally registered/designated properties that are to be protected. All buildings within the Heritage Area are automatically designated and subject to protection.



Potential Heritage Designation – buildings on properties graded as Level 1, which should be a priority for heritage designation. There may be other properties of lesser grade levels that may also warrant considered for designation.



Landmark View Plane – the iconic cone of view of Province House looking north on Great George Street. The heights of buildings within the view plane to the north of Province House should be restricted to ensure they do not come into view from the perspective of a pedestrian standing at any point between Dorchester Street and Richmond Street.



Landmark Street: Queen St

Landmark Streets – streets of landmark civic character due to their historic, symbolic, and primary pedestrian function. These streets also serve as important connections between key destinations. Buildings along these streets should exhibit the highest quality in architecture and materiality.



Civic Street: Kent Street

Civic Streets – streets that serve as important pedestrian connections between key destinations, open spaces and the waterfront. Buildings along these streets should exhibit a high level of quality, particularly at the level of the street, so as to enhance the pedestrian experience.



Frontages on Rochford Square

Key Civic Frontages – visually prominent building frontages that frame Landmark Streets, the five civic squares spaces and the waterfront. Due to their prominence, these frontages greatly shape the image and experience of the district and warrant the greatest attention to design, detail and material quality.



Gateway defined by architecture & landscape

Key Gateways – these are the primary or symbolic points of entry into the district, which should be reinforced through a combination of distinctive streetscaping, landscaping, signage and/ or architectural corner treatments.

3.0 urban design framework: civic elements



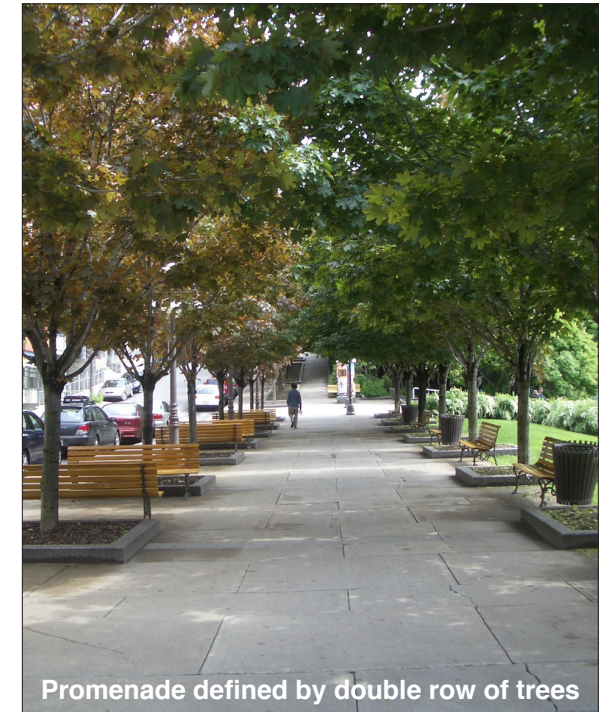
Key Views & Termini Sites – these view corridors terminate at key natural features, open spaces or building sites. Through public art, monuments or landmark architectural treatments, these termini sites provide an opportunity to celebrate important views while helping to orient and draw visitors throughout the district.



Key Corners – these sites correspond to the corners of key intersections and at gateways, which should be reinforced through high quality and distinctive architectural treatments.



Potential Open Spaces – potential or improved major publically accessible open spaces that serve to strengthen a connected network and that can include greens, plazas, recreational trail, boardwalks, multi-purpose surface parking areas, concession buildings and pavilions.



Potential Promenade – enhanced pedestrian paths to strengthen connections between or along streets and reinforced with a strong landscaping feature such as a double row of trees.



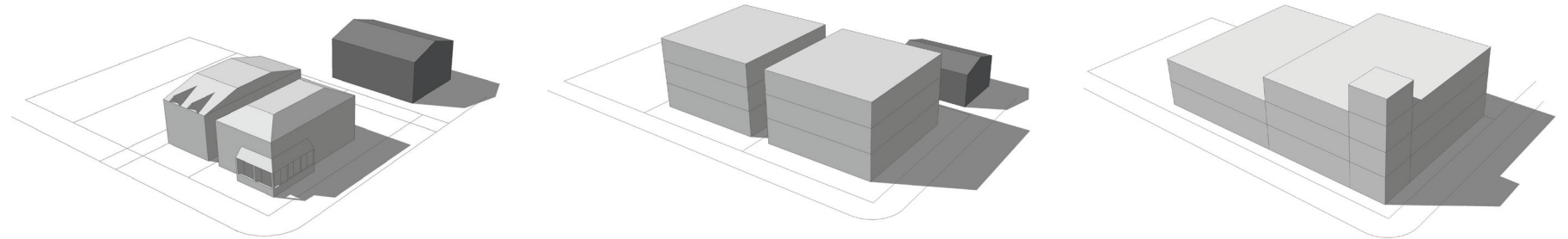
4.0 development standards

The following section sets out the recommended standards for development organized according to the five *Urban Character Areas* identified in Section 3.0 *Urban Design Framework*. These form-based standards have been tailored to the 500 Lot Area, emerging primarily from on-site analysis but also informed by the existing zoning standards to ensure that they do not result in any significant conflicts with respect to as-of-right development.

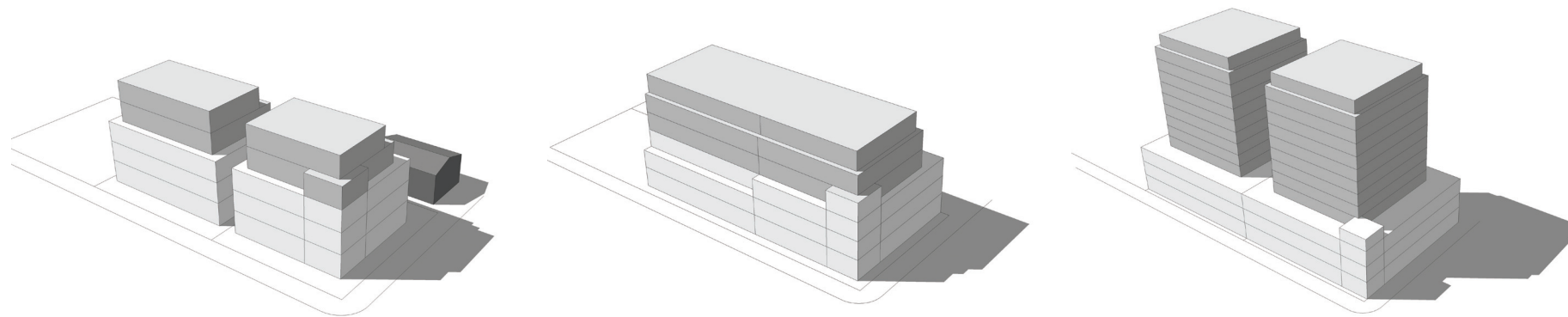
The *Development Standards* have been organized to easily supplement or replace the existing zoning standards, providing guidance for matters such as general uses, setbacks, and parking placement. As a form-based approach, these standards go further in providing guidance for appropriate building typologies and illustrative diagrams to describe intended built form outcomes. Furthermore, standards and conditions are set out for bonusing additional development in exchange for public benefits such as heritage protection.

As deemed necessary, some standards include explanation, which would not typically be included for in-force standards. This is to provide the rationale for certain standards and may be removed when the standards are implemented.

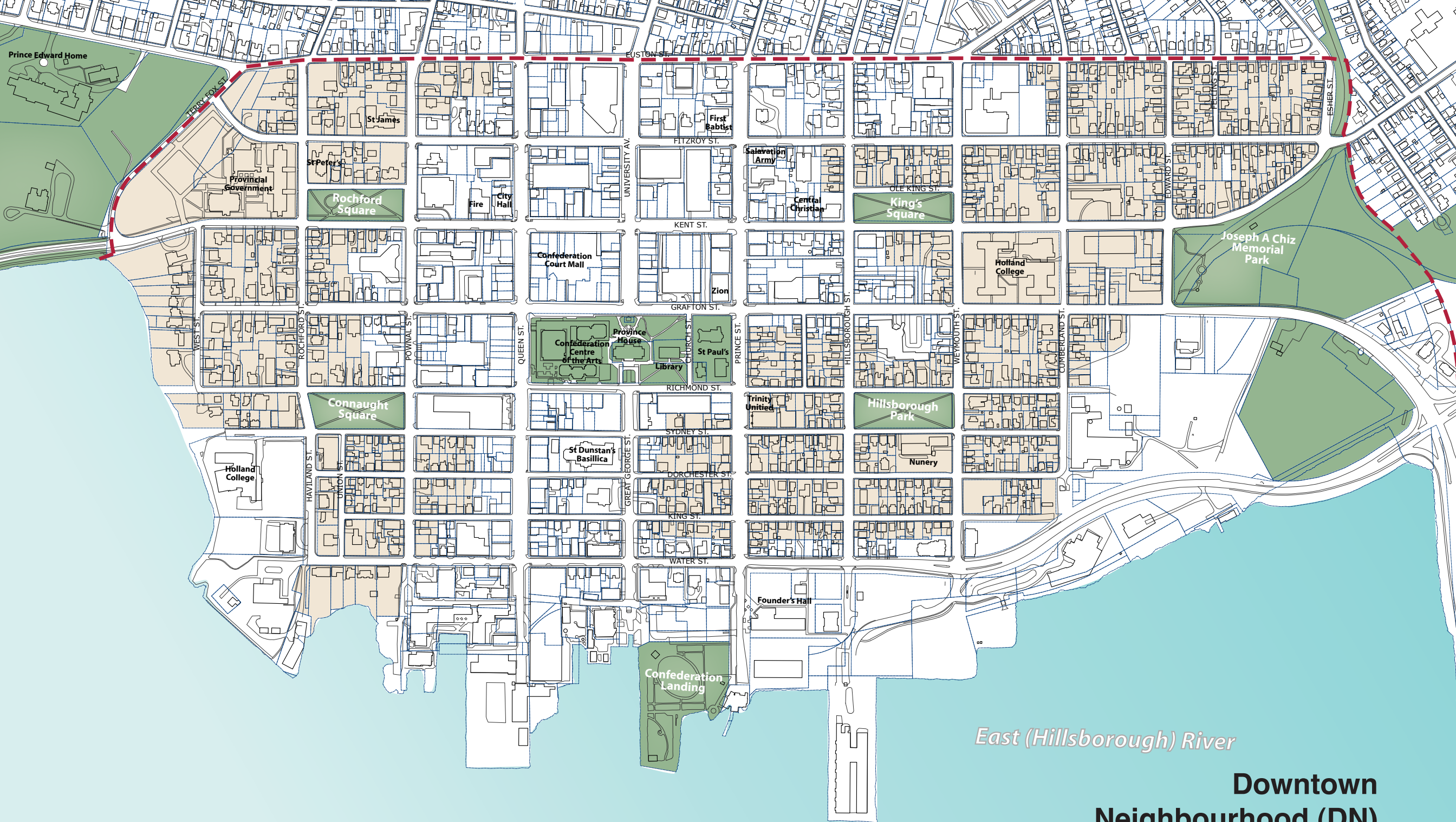
Given the undefined nature of the Waterfront and its future condition, its section assumes standards a yet to be defined combination of the other four *Urban Character Areas* may be adopted. These may be more clearly defined with a comprehensive plan. Accordingly, the primary direction is provided in the form of general guidelines for planning the area as a whole, rather than site-specific development standards.



Standards are provided for general development in neighbourhood and mixed-use areas



Standards are also provided for potential additional development permissions in the form of a bonus in exchange for public benefits



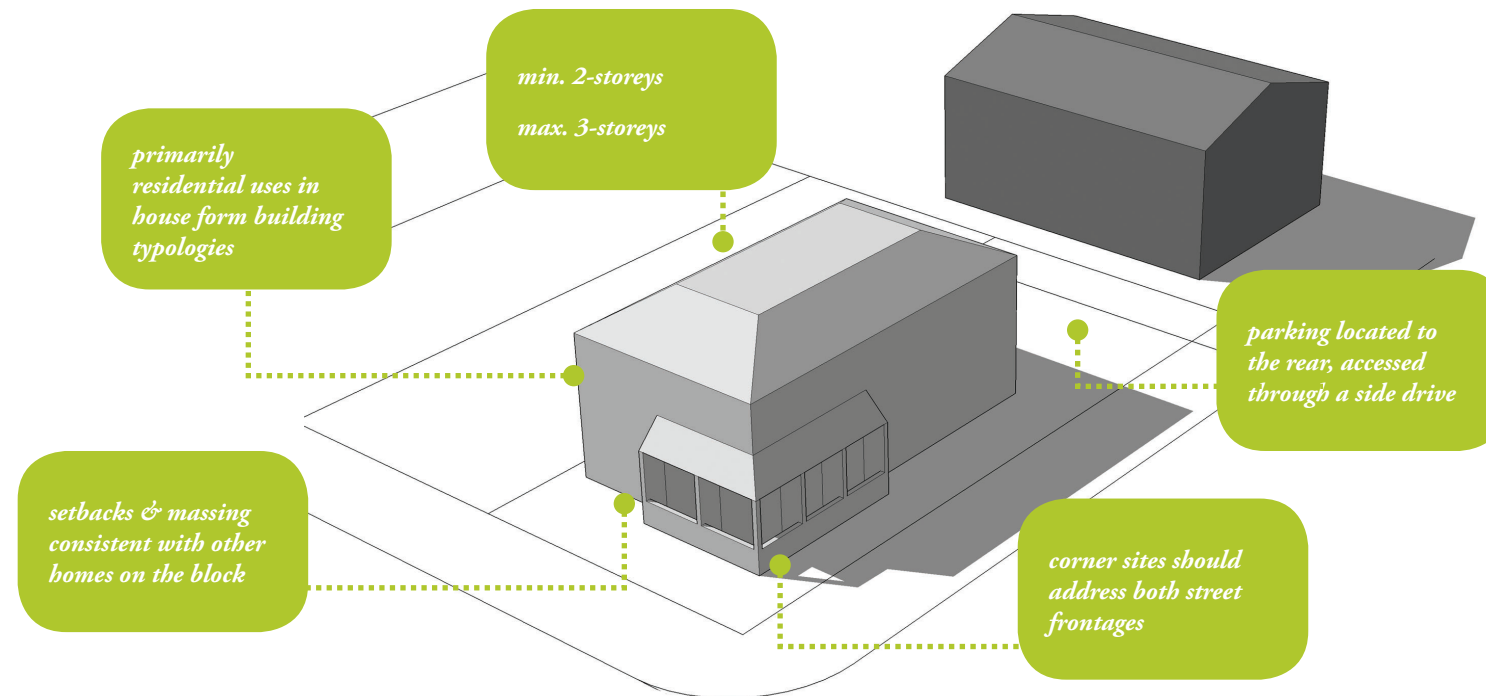
East (Hillsborough) River

Downtown Neighbourhood (DN)

4.1 Downtown Neighbourhood (DN)

Description

Downtown Neighbourhood (DN) areas have a prevailing residential use and character and are generally focused on the Rochford, Connaught, Hillsborough and King's Squares. These areas are defined by a variety of residential building types of differing architectural styles, but predominantly in 2-3 storey house forms such as detached, semi-detached and row housing. These areas also include some apartment forms and institutional buildings. Front-yard setbacks and porches vary depending on the street and on-site parking is located at the side or rear of the properties. Changes in these areas should be modest and primarily in the form of small-scale residential renovations, infill and additions that is consistent with the surrounding built character.



Key Development Objectives for House Forms



General Uses

- Park/Open Space
- Residential
- Nursing Home
- Institutional
- Bed & Breakfast
- Small-Scale Inns
- Small-Format Convenience Retail (corner store)
- Automobile-Oriented Use/Drive-thru Prohibited

Typical Building Types

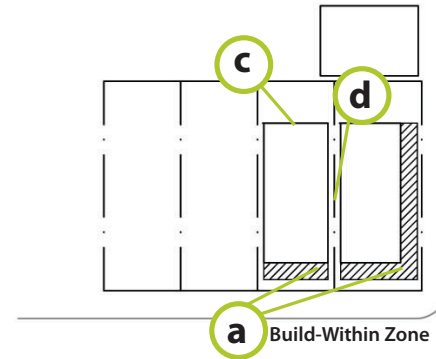
- Single-Detached
- Semi-Detached
- Duplex/Triple Decker
- Row House/Stacked Town
- Public/Institutional Forms

Minimum Lot Frontages

The minimum lot frontage corresponds to building type and shall be consistent with the average of the prevailing lot dimensions of existing properties in the surrounding blocks that contain similar building types. Otherwise, minimum frontages are as follows:

Buildings Type	Interior Lot	Corner Lot
Single-Detached	10.6m (35ft)	12.2m (40ft)
Semi-Detached	7.6m (25ft)	10.6m (35ft)
Duplex/Triple Decker	15.2m (50ft)	18.3m (60ft)
Row House/Stacked Town	6.1m (19.7ft)	9.0m (29.5ft)
Walk-up Apartment	25.0m (82ft)	30.0m (98.4ft)

Typical building placement for house forms



Principle Building Placement

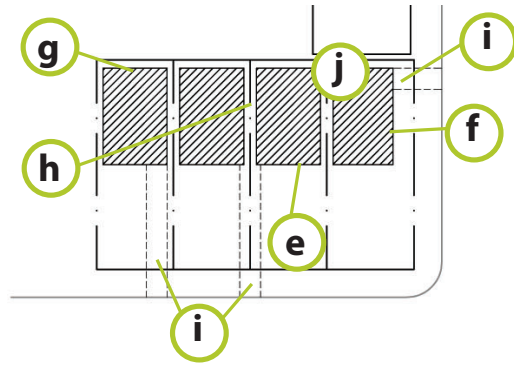
Principle buildings shall be placed within the shaded area as indicated in the figures and subject to the following standards:

- Front Yard/Flank Yard setback:** Any distance within the Build-Within Zone as established by the minimum and maximum front yard setbacks of the prevailing placement of existing buildings on the block that share the same street frontage. Otherwise, a minimum 2.4m (8ft) and maximum 4.9m (16ft).
- Front Yard Encroachments:** The type, scale, extent and setback from the street line of encroachments into the front yard setback of elements such as porches, stoops, and bay windows, should be consistent with the prevailing pattern of the existing buildings on the block sharing the same street frontage. Otherwise a maximum 2.4m (8ft) encroachment subject to a minimum 1.2m (4ft) setback from the street line.

- Rear Yard setback:** The average distance that existing buildings on the block are setback from the rear lot line or a minimum 6.0m (19.7ft), which ever is greater.
- Side Yard setback:** A distance equal to the side yard setback of the existing building on the abutting property, otherwise a minimum of 1.83m (6.0ft).

4.0 development standards: downtown neighbourhood

Typical placement for accessory buildings, parking & access for house forms



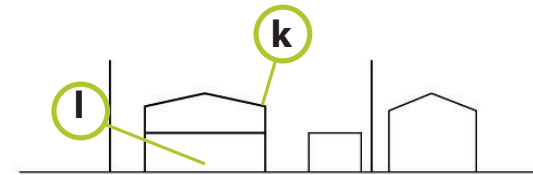
Accessory Buildings, Parking & Access

Accessory buildings and on-site parking shall be placed within the shaded area as indicated in the figures and subject to the following standards:

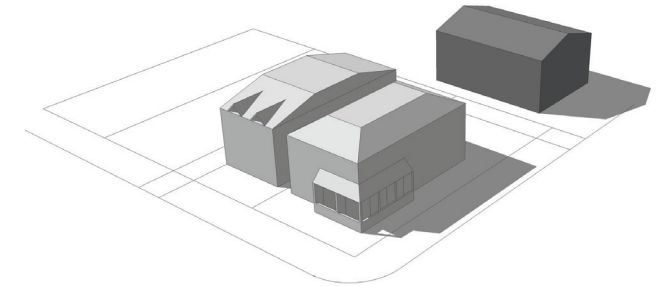
- e. **Front Yard setback:** The minimum being the distance equal to 50% of the lot.
- f. **Flank Yard setback:** Minimum 6.0m (19.7ft).
- g. **Rear Yard setback:** Minimum 1.2m (3.9ft).
- h. **Side Yard setback:** Minimum 1.83m (6ft) for accessory buildings. Minimum 0.6m (2ft) for parking.
- i. **Vehicular Access:** Permitted through a side driveway. Corner lots are permitted to provide a rear driveway accessed from the side street. Shared and consolidated driveways are encouraged where possible.

- j. **Car Garage:** Rear yard attached or detached garages are permitted. Front yard attached or detached garages that sit proud of the main building wall are prohibited. Side yard attached or detached garages are permitted where the width of the garage frontage is no greater than 50% of the width of principle building's frontage.

Typical profile for house forms



Typical massing for house forms

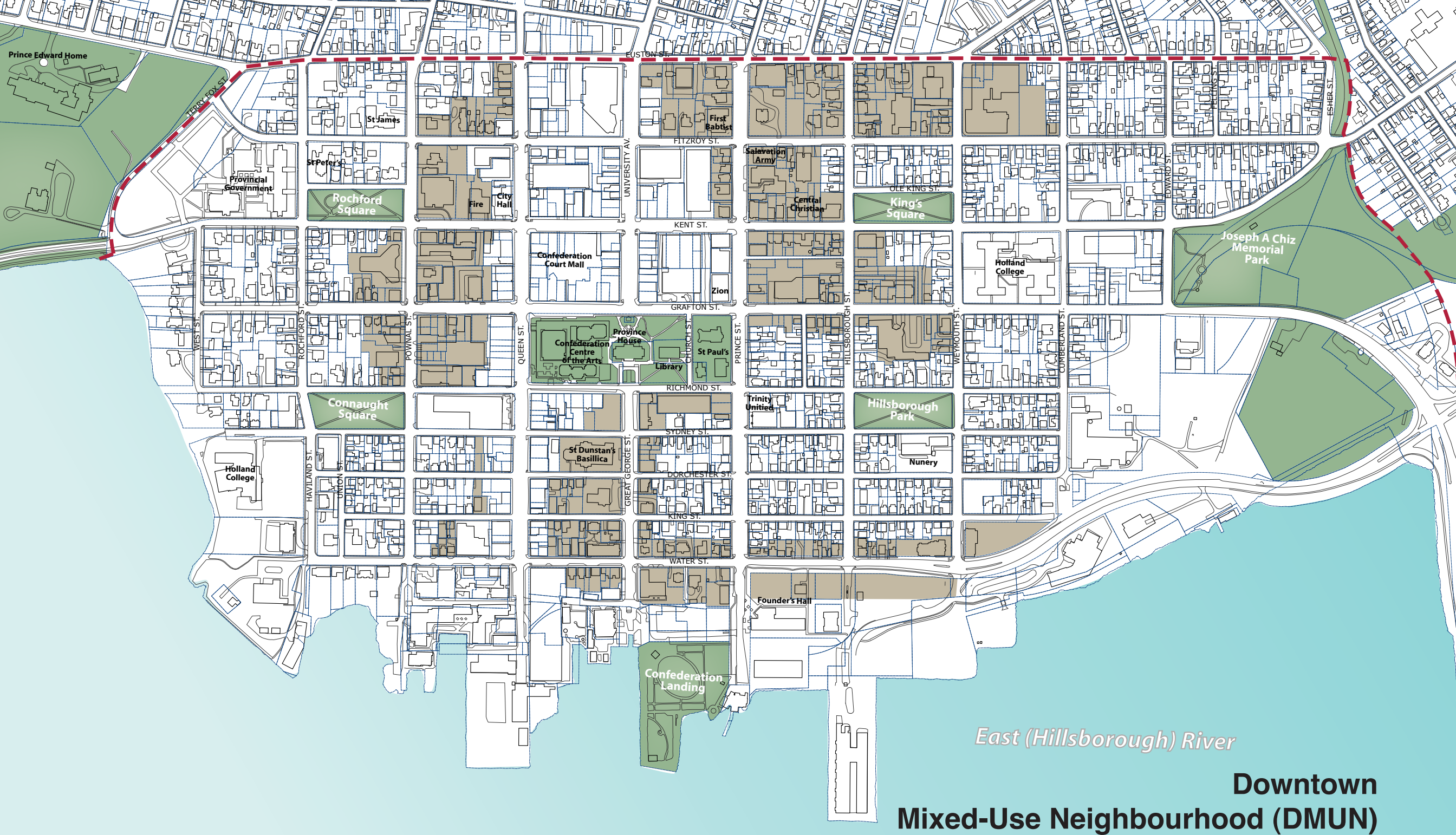


Building Height* & Massing

Structures shall comply with the following standards:

- k. **Building Height:** Minimum 2-storeys or 7.5m (24.6ft). Maximum 3-storeys or 12.2m (40ft) to peak of roof. Maximum 1-storey or 7.3m (24ft) for accessory buildings.
- l. **Grade Level Height:** Minimum 4.0m (13ft) grade to top of second floor height for residential uses, otherwise minimum 4.5m (14.8ft) floor-to-floor grade level height for non-residential.
- m. **Massing:** Building profiles, massing and rooflines should be consistent with the character of similar building types on the surrounding blocks and subject to the applicable design guidelines contained in Section 5.0.

*Heights shall be measured from a reference plane representing the average grade level of the property



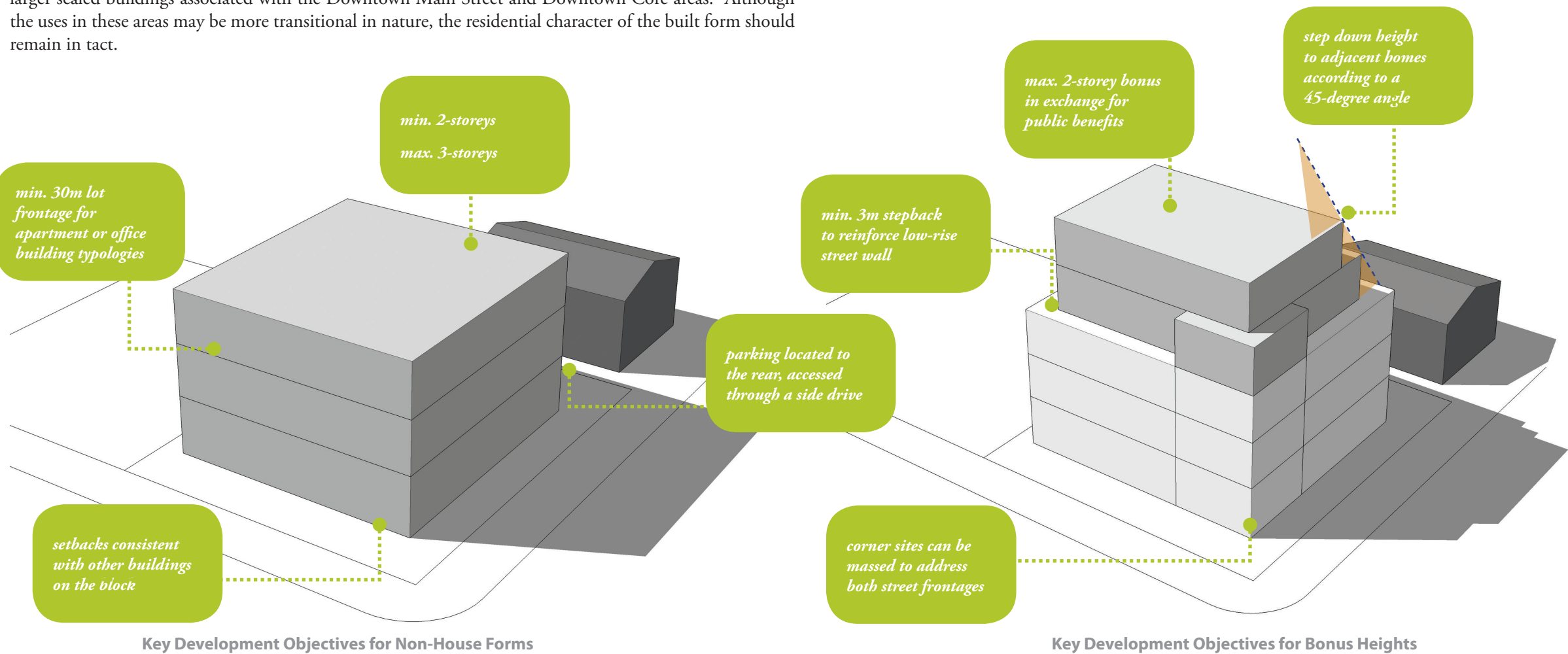
East (Hillsborough) River

**Downtown
Mixed-Use Neighbourhood (DMUN)**

4.2 Downtown Mixed-Use Neighbourhood (DMUN)

Description

Downtown Mixed-Use Neighbourhood (DMUN) areas are similar to Downtown Neighbourhoods in built character except for a slightly greater variety and mix of uses and building types and scales. Although the house-form building remains the predominant typology, many of these buildings have over time been readapted to accommodate mixed-uses (commercial and residential) or entirely non-residential uses. These areas generally provide a transition between the predominantly residential uses and the more intense and larger scaled buildings associated with the Downtown Main Street and Downtown Core areas. Although the uses in these areas may be more transitional in nature, the residential character of the built form should remain in tact.





General Uses

- Park/Open Space
- Residential
- Nursing Home
- Institutional
- Bed & Breakfast
- Small to Medium Scale Inns
- Small-Format Retail, Restaurants and Services
- Professional Offices
- Clinics
- Automobile-Oriented Use/Drive-thru Prohibited

Typical Building Types

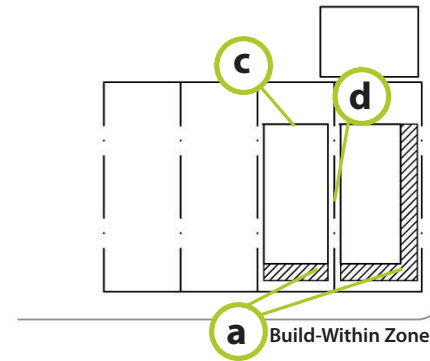
- Single-Detached
- Semi-Detached
- Duplex/Triple Decker
- Row House/Stacked Town
- Walk-up/Low-rise Apartment
- Small Format Office Building
- Public/Institutional Forms
- Above-Grade Parking Garage
- Pedestrian Bridges or Tunnels that pass over or under streets are Prohibited

Minimum Lot Frontages

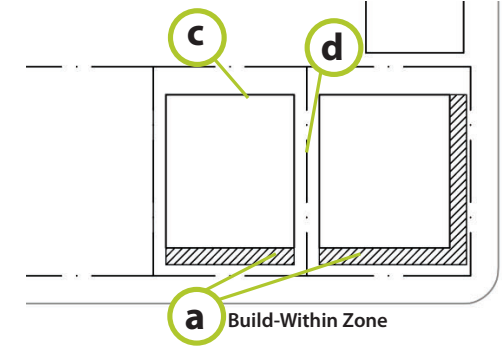
The minimum lot frontage corresponds to building type and shall be consistent with the average of the prevailing lot dimensions of existing properties in the surrounding blocks that contain similar building types, and that can achieve these standards. Otherwise, minimum frontages are as follows:

Buildings Type	Interior Lot	Corner Lot
Single-Detached	10.6m (35ft)	12.2m (40ft)
Semi-Detached	7.6m (25ft)	10.6m (35ft)
Duplex/Triple Decker	15.2m (50ft)	18.3m (60ft)
Row House/Stacked Town	6.1m (19.7ft)	9.0m (29.5ft)
Walk-up/Low-rise Apartment	30.0m (98.4ft)	30.0m (98.4ft)
Small Format Office Building	30.0m (98.4ft)	30.0m (98.4ft)
Above-Grade Parking Garage	25.0m (82ft)	30.0m (98.4ft)

Typical building placement for house forms



Typical building placement for non-house forms



Principle Building Placement

Principle buildings shall be placed within the shaded area as indicated in the figures and subject to the following standards:

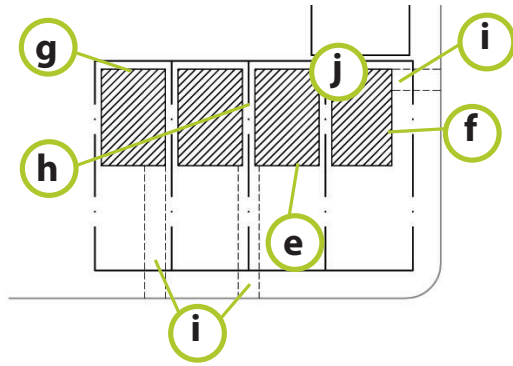
- Front Yard/Flank Yard setback:** Any distance within the Build-Within Zone as established by the minimum and maximum front yard setbacks of the prevailing placement of existing buildings on the block that share the same street frontage. Otherwise, a minimum 2.4m (8ft) and maximum 4.9m (16ft).
- Front Yard Encroachments:** The type, scale, extent and setback from the street line of encroachments into the front yard setback of elements such as porches, stoops, and bay windows, should be consistent with the prevailing pattern of the existing buildings on the block sharing the same street frontage. Otherwise a maximum 2.4m (8ft) encroachment subject to a minimum 1.2m (4ft) setback from the street line. Sidewalk cafes, including their furnishings, fencing, landscaping and awnings

are permitted to encroach into the street right-of-way subject to maintaining a minimum 1.8m (6ft) unobstructed, pedestrian clearway at the sidewalk.

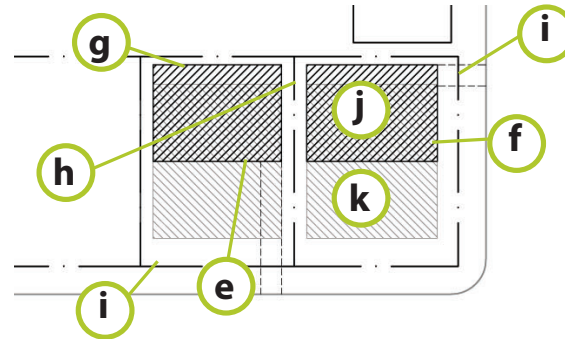
- Rear Yard setback:** Minimum 6.0m (19.7ft). Where abutting properties within the Downtown Neighbourhood area, the rear yard setback shall be the average distance that existing buildings on those abutting properties are setback from the rear lot line or a minimum 6.0m (19.7ft), which ever is greater.
- Side Yard setback:** A distance equal to the side yard setback of the existing building on the abutting property, otherwise a minimum of 1.83m (6.0ft). Not required for Row House/Stacked Town or where the building on the abutting property is not setback.

4.0 development standards: downtown mixed-use neighbourhood

Typical placement for accessory buildings, parking & access for house forms



Typical placement for accessory buildings, parking & access for non-house forms



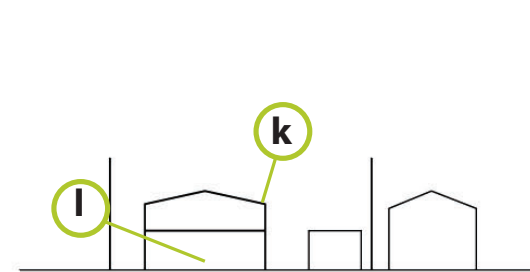
Accessory Buildings, Parking & Access

Accessory buildings and at-grade on-site parking shall be placed within the shaded area as indicated in the figures and subject to the following standards:

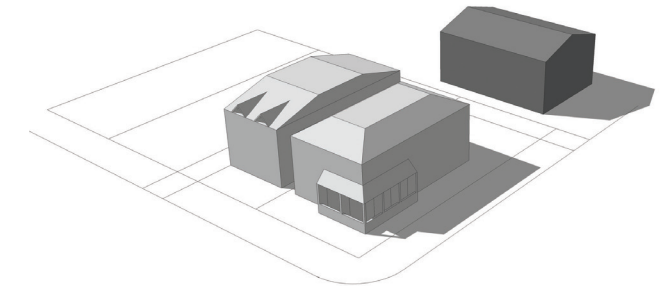
- e. **Front Yard setback:** The minimum being the distance equal to 30% of the lot depth, but no less than 15m (49.2ft).
- f. **Flank Yard setback:** Minimum 6.0m (19.7ft).
- g. **Rear Yard setback:** Minimum 1.2m (3.9ft).
- h. **Side Yard setback:** Minimum 1.83m (6ft) for accessory buildings. Minimum 0.6m (2ft) for parking.
- i. **Vehicular Access:** Permitted through a side driveway. Corner lots are permitted to provide a rear driveway accessed from the side street. Shared and consolidated driveways are encouraged where possible.

- j. **Car Garage:** Rear yard attached or detached garages are permitted. Front yard attached or detached garages that sit proud of the main building wall are prohibited. Side yard attached or detached garages are permitted where the width of the garage frontage is no greater than 50% of the width of principle building's frontage.
- k. **Above-Grade Public Parking Garage:** These public parking facilities are only permitted where they do not abut properties within a Downtown Neighbourhood area. Parking at the grade level is subject to the above parking placement setbacks to enable commercial, office or institutional uses fronting onto the street. Upper parking levels to a maximum height* of 12.5m (41ft) are subject to the setbacks for the Principle Building Placement and the applicable design guidelines in Section 5.0.

Typical profile for house forms



Typical massing for house forms

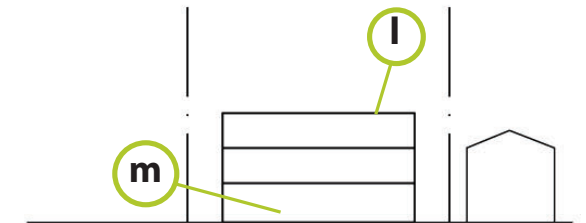


Building Height* & Massing

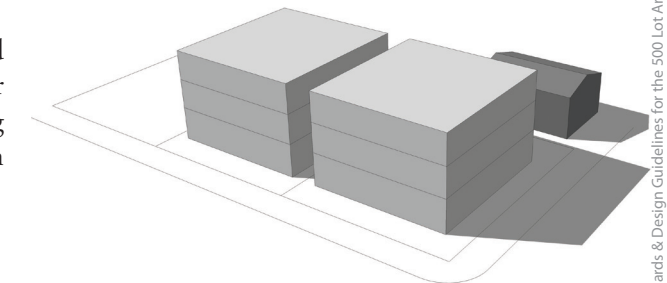
Structures shall comply with the following standards:

- l. **Building Height:** Minimum 2-storeys or 7.5m (24.6ft). Maximum 3-storeys or 12.2m (40ft) to peak of roof. Maximum 1-storey or 7.3m (24ft) for accessory buildings.
- m. **Grade Level Height:** Minimum 4.0m (13ft) grade to top of second floor height for residential uses, otherwise minimum 4.5m (14.8ft) floor-to-floor grade level height for non-residential.
- n. **Massing:** Building profiles, massing and rooflines should be consistent with the character of similar building types on the surrounding blocks and subject to the applicable design guidelines contained in Section 5.0.

Typical profile for non-house forms

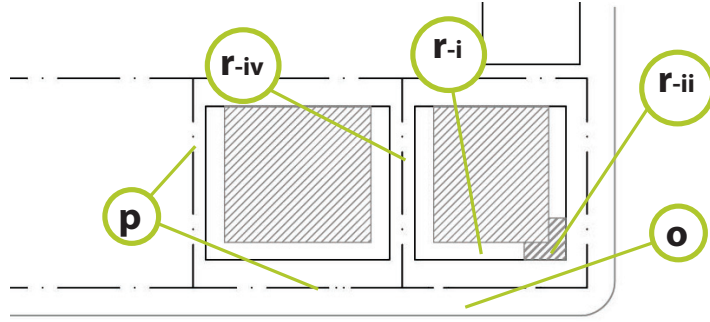


Typical massing for non-house forms

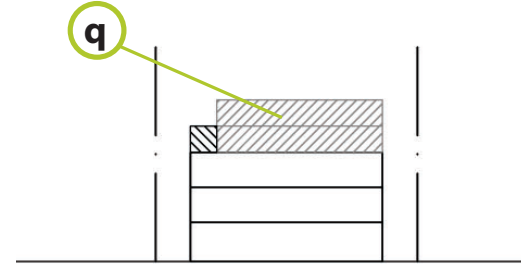


*Heights shall be measured from a reference plane representing the average grade level of the property

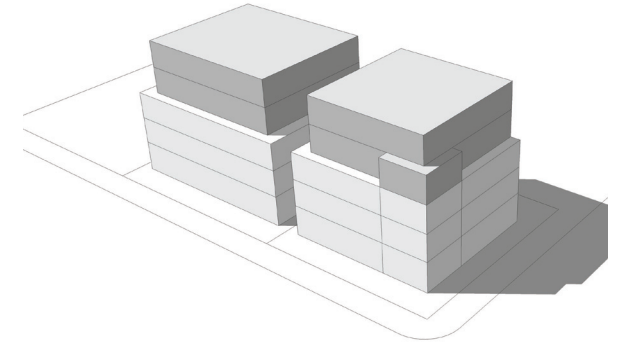
Typical placement for bonus height



Typical profile for bonus height



Typical massing for bonus height



Bonus Height* & Massing

A number of buildings in the Downtown Mixed-Use Neighbourhood areas exceed the 3-storey height limit. Accordingly, where appropriate and desirable, a bonus height may be considered in exchange for securing specific public benefits such as heritage retention, affordable housing, or cultural and community facilities (see Section 6.0 for recommendations on a bonus formula). Bonus heights are subject to the following development standards:

- o. **Principle Street Frontage:** Only properties with a principle frontage on either a street right-of-way or open space of 60ft or greater are eligible for a height bonus. This is to avoid a ‘canyon’ effect by ensuring that building heights are proportional to, or do not exceed, the width of the street or open space they frame.

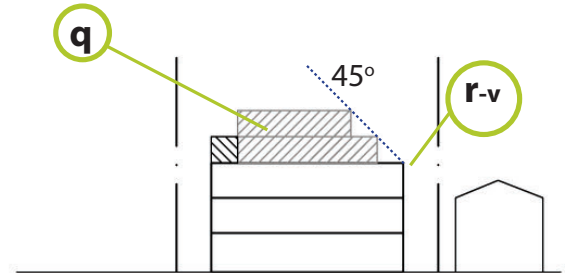
- p. **Lot Dimensions:** Minimum frontage of 30.0m (98.4ft) for interior lots and 25.0m (82ft) for corner lots. Minimum depth of 30.0m (98.4ft). This is to ensure an adequate lot size for a viable development that can be appropriately designed and massed to mitigate the additional height while providing all other necessary elements such as on-site parking and amenities.

- q. **Bonus Height:** A bonus of up to 2-storeys may be considered for a maximum building height of 5-storeys or 18.5m (60.7ft).

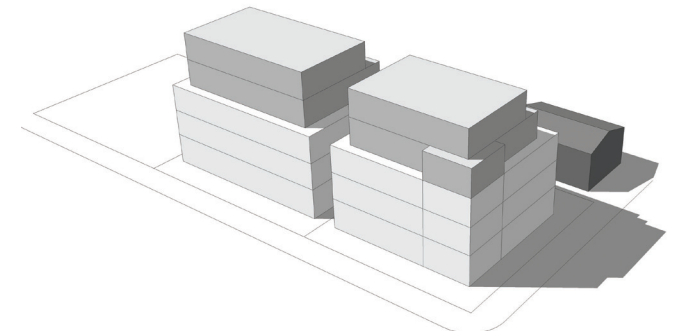
- r. **Massing:** The components above the base building that are a bonus in height shall be subject to:

- i. A minimum 3.0m (9.8ft) step-back from the building’s 2 or 3-storey street wall. This is to reinforce the low-rise street wall that characterizes the Downtown Neighbourhood and Mixed-Use Neighbourhood areas.
- ii. Subject to the Design Guidelines in Section 5.0, sites identified as Key Corner Sites are not required to provide the step-back at the corner of the building for a length of 6.0m (19.7ft) along either street frontage for a maximum height of 4-storeys or 16.5m (54.1ft).

Typical profile for bonus height where abutting a residential home



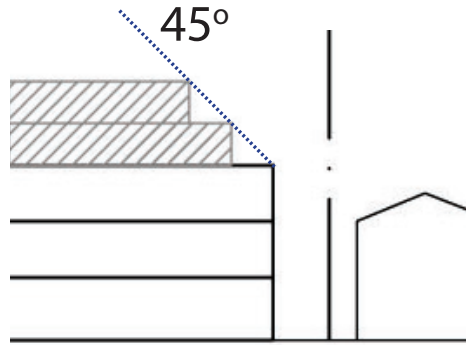
Typical massing for bonus height where abutting a residential home



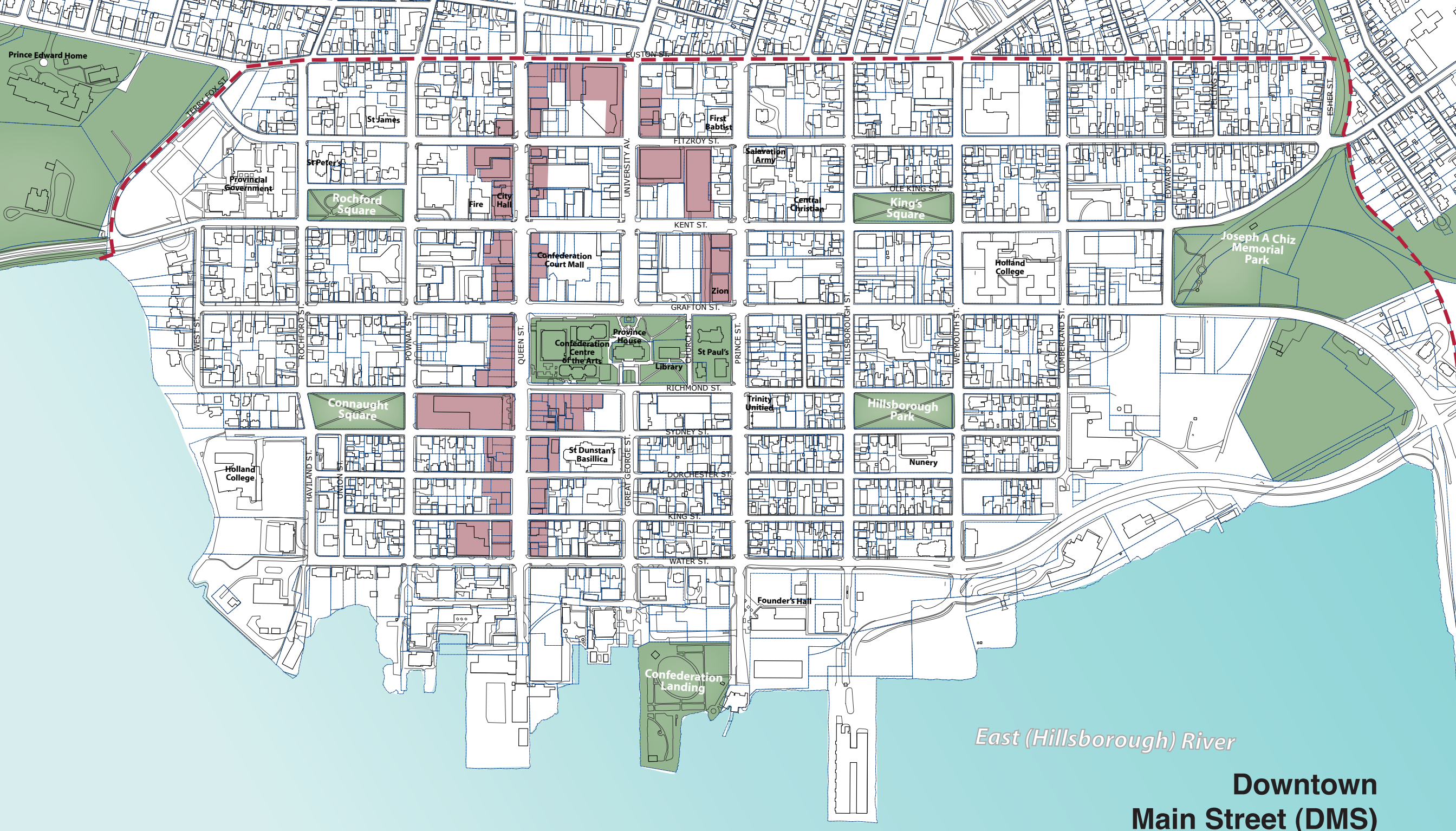
*Heights shall be measured from a reference plane representing the average grade level of the property

4.0 development standards: downtown mixed-use neighbourhood

Application of a 45-degree angular plane where abutting a residential home



- iii. For a bonus height greater than one storey, a minimum 6.0m (19.7ft) step-back is required from any retained and integrated heritage structures on the property, in addition to being consistent with all other applicable heritage related policies and design guidelines.
- iv. A minimum 5.5m (18ft) side yard setback or step-back to ensure adequate separation distances of the upper levels from adjacent properties that may potentially be eligible for a height bonus.
- v. 45-degree angular planes originating from the top of the flank or rear façade of the base building that face abutting properties that are low-rise residential homes or within a Downtown Neighbourhood area. This is to ensure a visible and adequate stepping down of height to adjacent low-rise residential properties.
- s. **Supporting Studies:** to be determined on a case-by-case basis, additional studies and analysis may be required to ensure that the additional height and/or its massing are meeting the desired performance standards with respect to mitigating visual, shadow, wind, traffic and heritage impacts.
- t. **Design Quality:** Developments considered for a height bonus shall demonstrate, to the satisfaction of the City, the highest possible standards in architectural, landscaping and material quality that is consistent with all applicable design guidelines in Section 5.0.



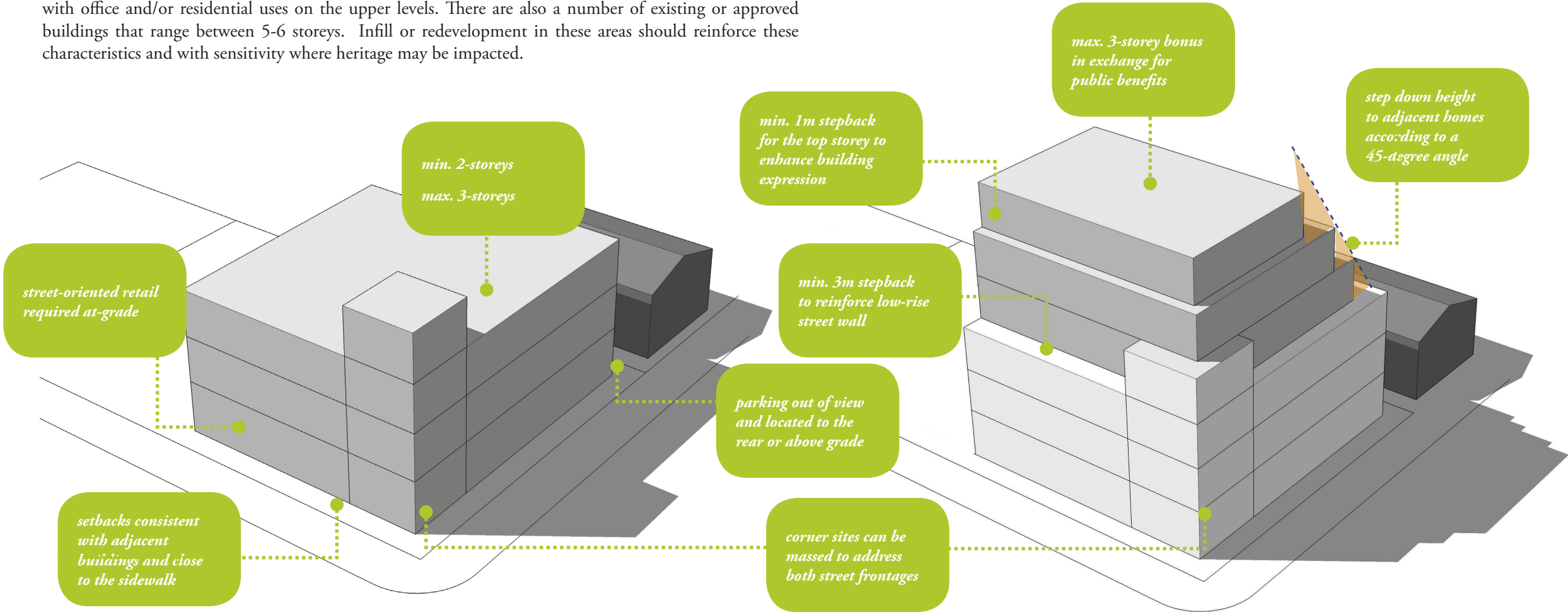
East (Hillsborough) River

Downtown Main Street (DMS)

4.3 Downtown Main Street (DMS)

Description

Downtown Main Street (DMS) areas are the traditional, retail-oriented, mixed-use corridors generally associated with Queen Street and University Avenue. These areas are predominately defined by 2-4 storey continuous buildings placed close the street edge and consisting of narrow storefronts at the street level, with office and/or residential uses on the upper levels. There are also a number of existing or approved buildings that range between 5-6 storeys. Infill or redevelopment in these areas should reinforce these characteristics and with sensitivity where heritage may be impacted.



Key Development Objectives

Key Development Objectives for Bonus Heights



General Uses

- Park/Open Space
- Small to Medium Format Street-Oriented Retail required at grade on Queen Street and University Avenue
- Mixed-Uses (above grade residential)
- Variety of Office/Commercial
- Cultural and Entertainment
- Professional Offices
- Hotels and Inns
- Clinics
- Institutional
- Automobile-Oriented Use/Drive-thru Prohibited

Typical Building Types

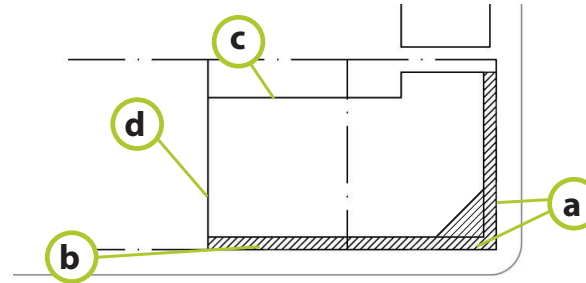
- Main Street/Street Wall Forms
- Low to Mid-Rise Apartment Forms
- Low to Mid-Rise Office Buildings
- Public/Institutional Forms
- Above-Grade Parking Garage
- Pedestrian Bridges or Tunnels that pass over or under streets are Prohibited

Minimum Lot Frontages

The minimum lot frontage corresponds to building type and shall be consistent with the average of the prevailing lot dimensions of existing properties in the surrounding blocks that contain similar building types, and that can achieve viable forms consistent with these standards. Otherwise, minimum frontages are as follows:

Buildings Type	Interior Lot	Corner Lot
Main Street/Street Wall Forms	6.1m (20ft)	6.1m (20ft)
Low to Mid-Rise Apartment Form	30.0m (98.4ft)	25.0m (82ft)
Low to Mid-Rise Office Buildings	30.0m (98.4ft)	25.0m (82ft)
Above-Grade Parking Garage	25.0m (82ft)	20.0m (65.6ft)

Typical building placement where abutting a residential home



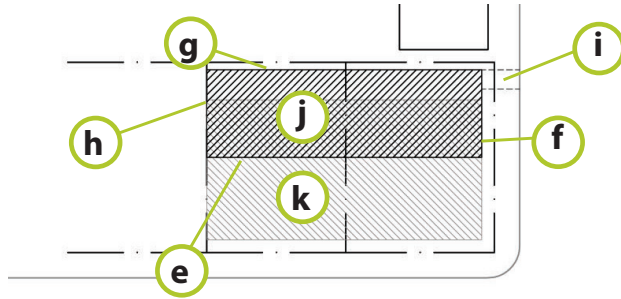
Principle Building Placement

Principle buildings shall be placed within the shaded area as indicated in the figures and subject to the following standards:

- Front Yard/Flank Yard setback:** Any distance within the Build-Within Zone as established by the minimum and maximum front yard setbacks of the prevailing placement of existing buildings on the block that share the same street frontage. Otherwise, a minimum 0.0m (0ft) and maximum 1.0m (3.3ft). Corner properties and full block developments may setback a maximum 2.0m (6.6ft). At corner sites, the maximum setback for a chamfered corner is 2.0m (6.6ft) as measured from the corner of the property to the face of the chamfered building wall.
- Front Yard Encroachments:** The type, scale, extent and setback from, or projection over, the street line of above grade encroachments such as awnings or banner signs, should be consistent with the prevailing pattern of the existing buildings on the block sharing the same street frontage. Otherwise, a maximum of 1.5m (4.9ft) encroachment over the front yard setback and/or street right-of-way. Sidewalk cafes, including their furnishings, fencing, landscaping and awnings are permitted to encroach into the street right-of-way subject to maintaining a minimum 1.8m (6ft) unobstructed, pedestrian clearway at the sidewalk.
- Rear Yard setback:** Minimum 6.0m (19.7ft) where abutting properties containing heritage house forms or low-rise residential properties within the Downtown Neighbourhood and Mixed-Use Neighbourhood areas. Where on a corner site flanking a heritage house form or low-rise residence, a distance equal to the side yard setback of the existing building on the property abutting the rear property line and for a depth no greater than the depth of that building. Otherwise a rear yard setback is not required.
- Side Yard setback:** Prohibited unless associated with a publically accessible public space or pedestrian passage.

4.0 development standards: downtown main street

Typical parking placement & access where abutting a residential home



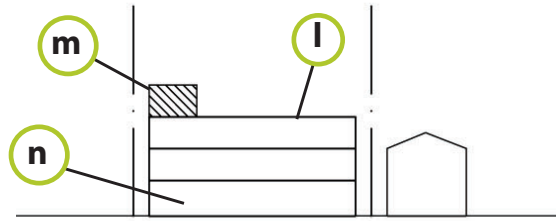
Parking Placement & Access

On-site parking is permitted below and above-grade, or to the rear where at-grade. At-grade parking shall be placed within the shaded area as indicated in the figures and subject to the following standards:

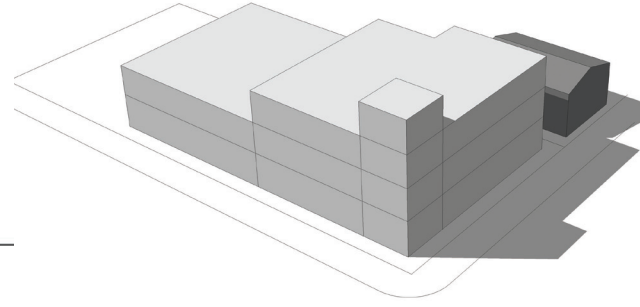
- e. **Front Yard setback:** The minimum being the distance equal to 30% of the lot depth, but no less than 15m (49.2ft).
- f. **Flank Yard setback:** Minimum 6.0m (19.7ft).
- g. **Rear Yard setback:** Minimum 1.2m (3.9ft) where abutting a Downtown Neighbourhood or Mixed-Use Neighbourhood. Otherwise not required.
- h. **Side Yard setback:** Minimum 0.6m (2ft) where abutting a Downtown Neighbourhood or Mixed-Use Neighbourhood. Otherwise not required.
- i. **Vehicular Access:** Front driveway access will only be permitted for properties with frontages of 50.0m (164ft) or greater. Adjacent property owners are encouraged to coordinate and consolidate vehicular access points, as only one front drive access shall be permitted on any given block. Corner lots are permitted to provide a rear driveway accessed from the side street. To minimize breaks in the street wall, vehicular access points shall be architecturally integrated within the street wall.
- j. **Above-Grade Parking:** Parking at the grade level is subject to the above parking placement setbacks to enable commercial or institutional uses fronting onto the street. Upper parking levels to a maximum height* of 12.5m (41ft) are subject to the setbacks for the Principle Building Placement and the applicable design guidelines in Section 5.0.
- k. **Above-Grade Public Parking Garage:** These public parking facilities are only permitted where they do not abut properties within a Downtown Neighbourhood area. Parking at the grade level is subject to the above parking placement setbacks to enable commercial, office or institutional uses fronting onto the street. Upper parking levels to a maximum height* of 12.5m (41ft) are subject to the setbacks for the Principle Building Placement and the applicable design guidelines in Section 5.0.

**Heights shall be measured from a reference plane representing the average grade level of the property*

Typical building profile



Typical massing



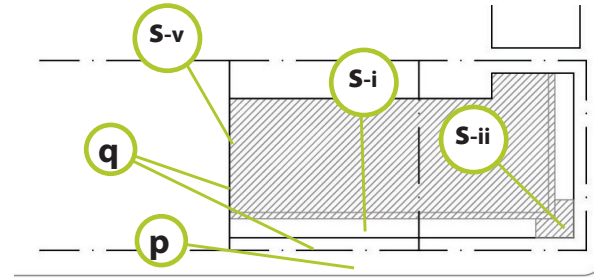
Building Height* & Massing

Structures shall comply with the following standards:

- l. **Building Height:** Minimum 2-storeys or 7.5m (24.6ft). Maximum 3-storeys or 12.5m (41ft).
- m. **Key Corner Sites:** Subject to the Design Guidelines in Section 5.0, the corner of the building for a length of 6.0m (19.7ft) along either street frontage may exceed the height limit by 1-storey or 4.0m (13.1ft) to a maximum height of 16.5m (54.1ft).
- n. **Grade Level Height:** Minimum 4.5m (14.8ft) floor-to-floor grade level height.
- o. **Massing:** Building profiles, massing and rooflines should be consistent with the character of similar building types on the surrounding blocks and subject to the applicable design guidelines contained in Section 5.0.

*Heights shall be measured from a reference plane representing the average grade level of the property

Typical plan for bonus height placement where abutting a residential home



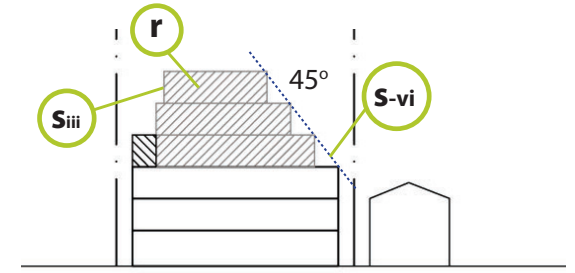
Bonus Height* & Massing

A significant number of existing and approved buildings in the Downtown Main Street area exceed the 3-storey height limit. Accordingly, where appropriate and desirable, a bonus height may be considered in exchange for securing specific public benefits such as heritage retention, affordable housing, or cultural and community facilities (see Section 6.0 for recommendations on a bonus formula). Bonus heights are subject to the following development standards:

- p. **Principle Street Frontage:** Only properties with a principle frontage on either a street right-of-way or open space of 80ft or greater are eligible for a height bonus. This is to avoid a 'canyon' effect by ensuring that building heights are proportional to, or do not exceed, the width of the street or open space they frame.

*Heights shall be measured from a reference plane representing the average grade level of the property

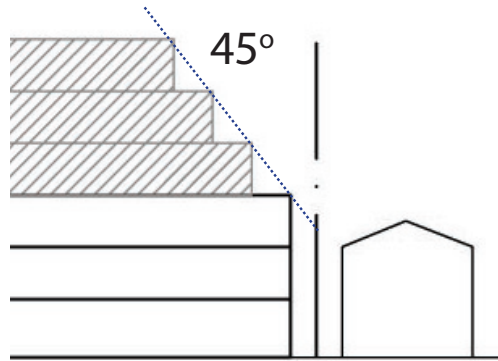
Typical profile for bonus height where abutting a residential home



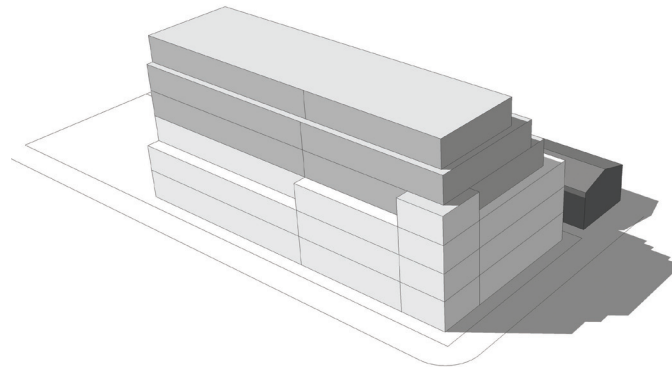
- q. **Lot Dimensions:** Minimum frontage of 30.0m (98.4ft) for interior lots and 25.0m (82ft) for corner lots. Minimum depth of 30.0m (98.4ft). This is to ensure an adequate lot size for a viable development that can be appropriately designed and massed to mitigate the additional height while providing all other necessary elements such as on-site parking and amenities.

4.0 development standards: downtown main street

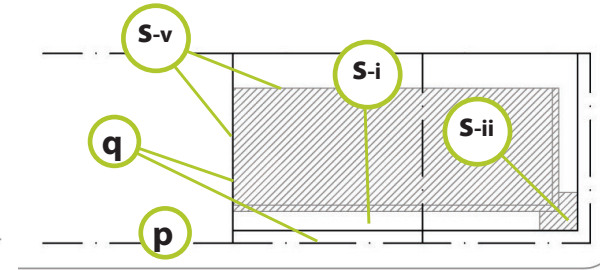
Application of a 45-degree angular plane where abutting a residential home



Typical massing for bonus height where abutting a residential home



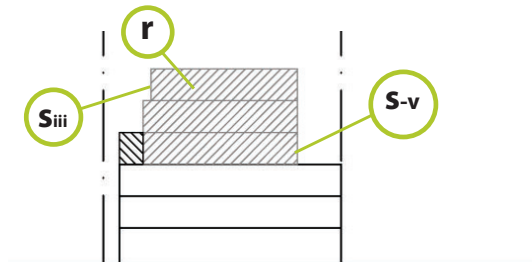
Typical placement for bonus height where abutting mixed-uses



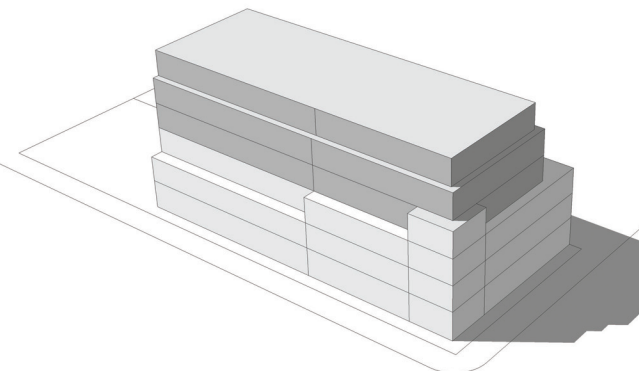
- r. **Bonus Height:** A bonus of up to 3-storeys may be considered for a maximum building height of 6-storeys or 24.5m (80.4ft).
- s. **Massing:** The components above the base building that are a bonus in height shall be subject to:
 - i. A minimum 3.0m (9.8ft) step-back from the building's 2 or 3-storey street wall. This is to reinforce the low-rise street wall that characterizes the Downtown Main Street area.
 - ii. Subject to the Design Guidelines in Section 5.0, sites identified as Key Corner Sites are not required to provide the step-back at the corner of the building for a length of 6.0m (19.7ft) along either street frontage for a maximum height of 4-storeys or 16.5m (54.1ft).
 - iii. A minimum 1.0m (3.3ft) step-back for the top storey to enhance building expression.

- iv. For a bonus height greater than one storey, a minimum 6.0m (19.7ft) step-back is required from any retained and integrated heritage structures on the property, in addition to being consistent with all other applicable heritage related policies and design guidelines.
- v. A minimum 5.5m (18ft) rear yard setback and 5.5m (18ft) side yard step-back where the building frontage exceeds 50.0m (164ft) so as to provide a visual break and to enable glazing on rear and sidewalls with adequate separation distances.
- vi. 45-degree angular planes originating from the top of the flank or rear façade of the base building that face abutting properties that are low-rise residential homes or within a Downtown Neighbourhood area. This is to ensure a visible and adequate stepping down of height to adjacent low-rise residential properties. Architectural features associated with civic landmarks and Key Corner Sites are not subject to the angular plane.

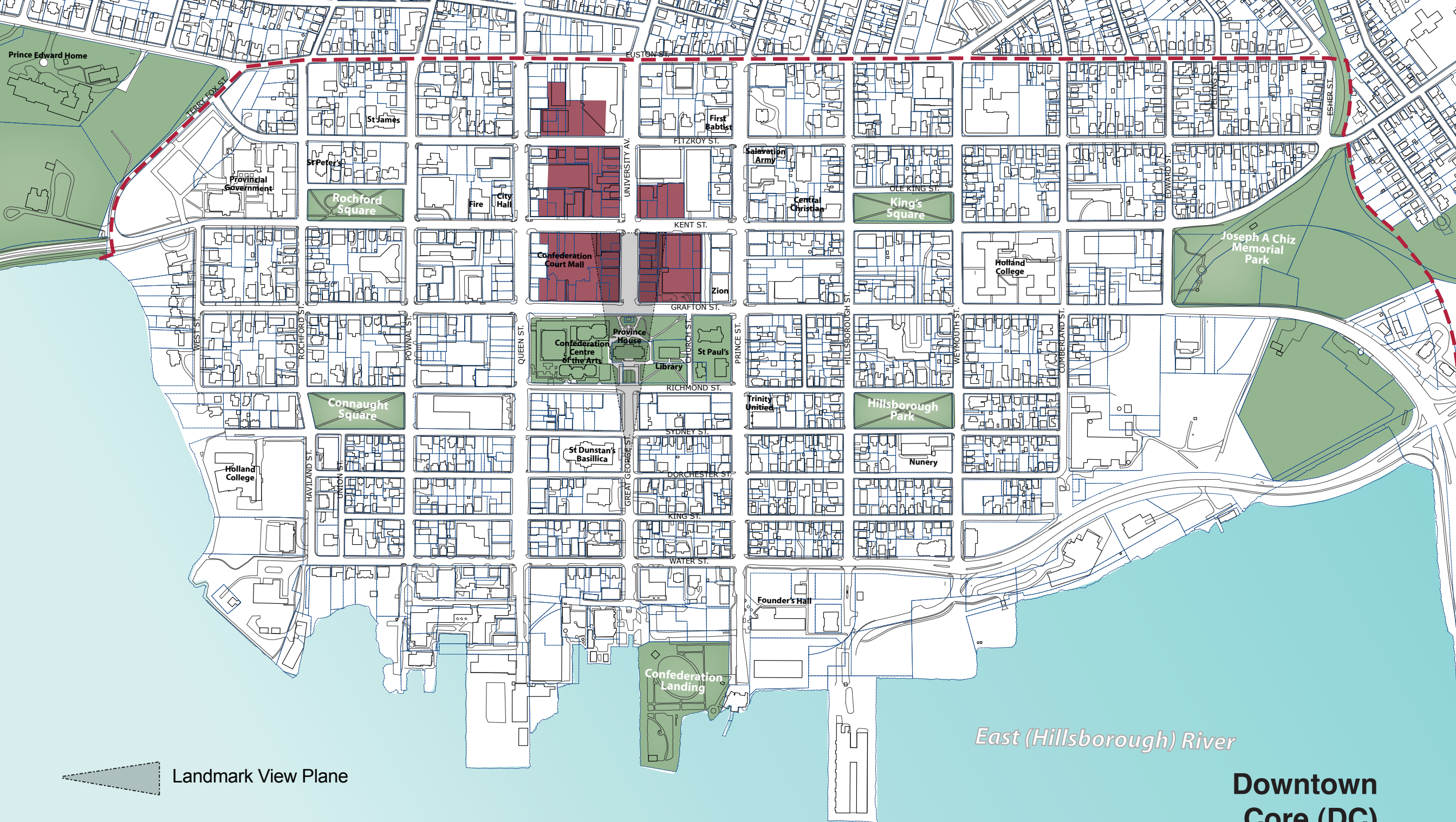
Typical placement for bonus height where abutting mixed-uses



Typical massing for bonus height where abutting mixed-uses



- t. **Supporting Studies:** to be determined on a case-by-case basis, additional studies and analysis may be required to ensure that the additional height and/or its massing are meeting the desired performance standards with respect to mitigating visual, shadow, wind, traffic and heritage impacts.
- u. **Design Quality:** Developments considered for a height bonus shall demonstrate, to the satisfaction of the City, the highest possible standards in architectural, landscaping and material quality that is consistent with all applicable design guidelines in Section 5.0.



Prince Edward Home

TERRY FOX ST.

Provincial Government

St James

St Peter's

Rochford Square

Fire

City Hall

Confederation Court Mall

Zion

Confederation Centre of the Arts

Province House

Library

St Paul's

Connaught Square

Holland College

St Dunstan's Basilica

Trinity United

Hillsborough Park

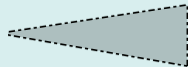
Nunery

Confederation Landing

Founder's Hall

East (Hillsborough) River

Downtown Core (DC)

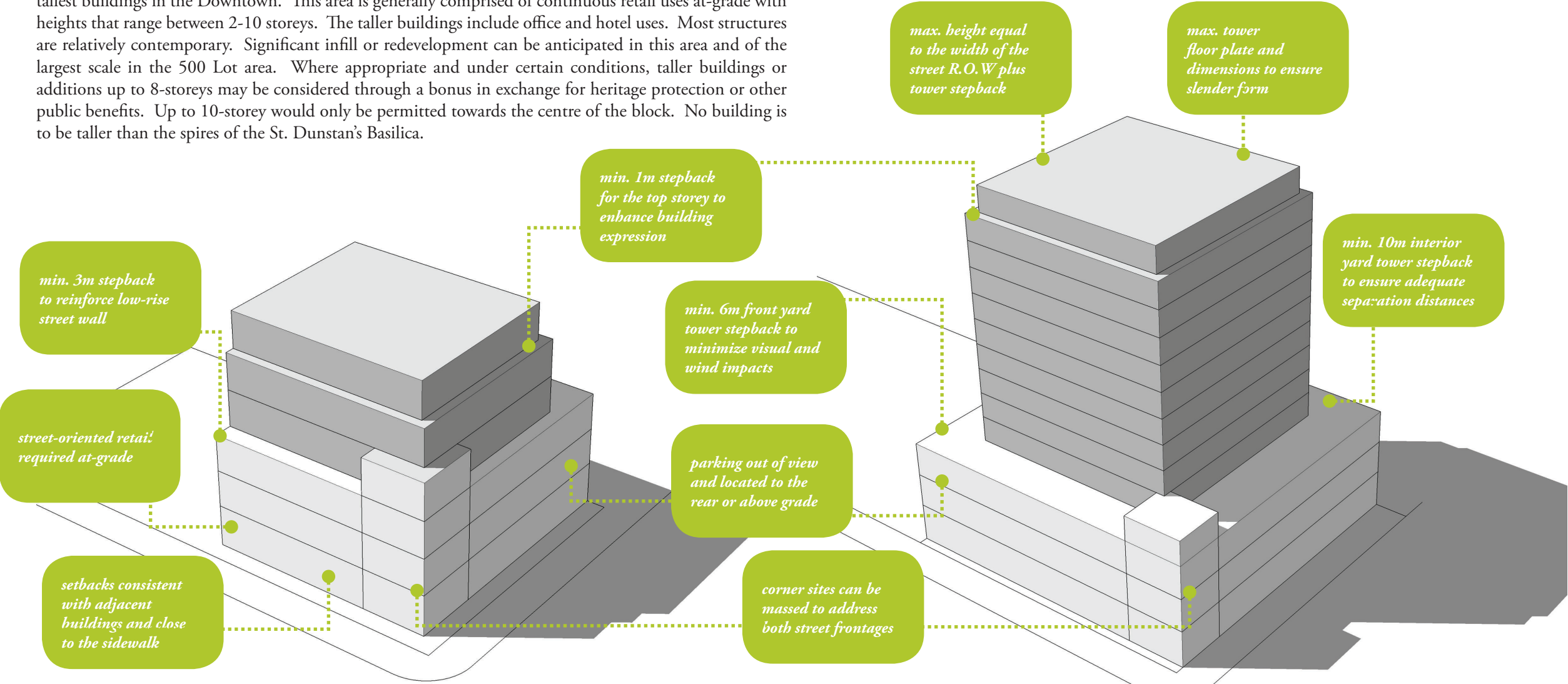


Landmark View Plane

4.4 Downtown Core (DC)

Description

Downtown Core (DC) defines the area with the greatest intensity of commercial and office use and the tallest buildings in the Downtown. This area is generally comprised of continuous retail uses at-grade with heights that range between 2-10 storeys. The taller buildings include office and hotel uses. Most structures are relatively contemporary. Significant infill or redevelopment can be anticipated in this area and of the largest scale in the 500 Lot area. Where appropriate and under certain conditions, taller buildings or additions up to 8-storeys may be considered through a bonus in exchange for heritage protection or other public benefits. Up to 10-storey would only be permitted towards the centre of the block. No building is to be taller than the spires of the St. Dunstan's Basilica.



Key Development Objectives for Bonus Heights: Buildings up to 6-Storeys

Key Development Objectives for Bonus Heights: Buildings Taller Than 6-Storeys



General Uses

- Park/Open Space
- Small to Medium Format Street-Oriented Retail required at grade
- Larger format retail directed internal to the blocks or second levels
- Mixed-Uses (above grade residential)
- Variety of Office/Commercial
- Cultural and Entertainment
- Professional Offices
- Hotels and Inns
- Clinics
- Institutional
- Automobile-Oriented Use/Drive-thru are Prohibited

Typical Building Types

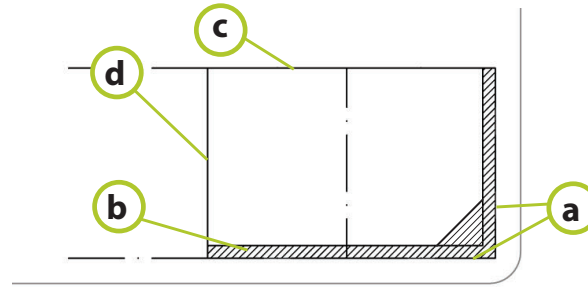
- Main Street/Street Wall Forms
- Mid-Rise Apartment Forms
- Mid-Rise Office Buildings
- Point Tower Form
- Public/Institutional Forms
- Above-Grade Parking Garage
- Pedestrian Bridges or Tunnels that pass over or under streets are Prohibited

Minimum Lot Frontages

The minimum lot frontage corresponds to building type and shall be consistent with the average of the prevailing lot dimensions of existing properties in the surrounding blocks that contain similar building types, and that can achieve viable forms consistent with these standards. Otherwise, minimum frontages are as follows:

Buildings Type	Interior Lot	Corner Lot
Main Street/Street Wall Forms	6.1m (20ft)	6.1m (20ft)
Mid-Rise Apartment Form	30.0m (98.4ft)	25.0m (82ft)
Mid-Rise Office Buildings	30.0m (98.4ft)	25.0m (82ft)
Point Tower Form	36.0m (118ft)	36.0m (118ft)
Above-Grade Parking Garage	25.0m (82ft)	20.0m (65.6ft)

Typical building placement



Principle Building Placement

Principle buildings shall be placed within the shaded area as indicated in the figures and subject to the following standards:

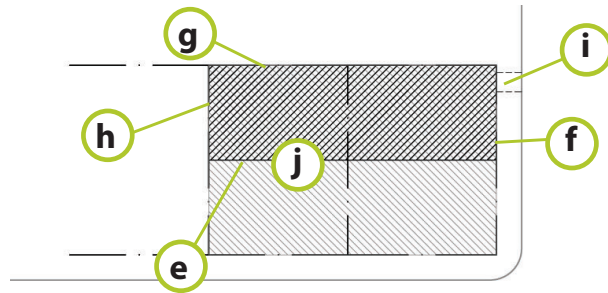
- Front Yard/Flank Yard setback:** Any distance within the Build-Within Zone as established by the minimum and maximum front yard setbacks of the prevailing placement of existing buildings on the block that share the same street frontage. Otherwise, a minimum 0.0m (0ft) and maximum 1.0m (3.3ft). Corner properties and full block developments may setback a maximum 2.0m (6.6ft). At corner sites, the maximum setback for a chamfered corner is 2.0m (6.6ft) as measured from the corner of the property to the face of the chamfered building wall.
- Front Yard Encroachments:** The type, scale, extent and setback from, or projection over, the street line of above grade encroachments such as awnings or banner signs, should be consistent with the prevailing pattern of the existing

buildings on the block sharing the same street frontage. Otherwise, a maximum of 1.5m (4.9ft) encroachment over the front yard setback and/or street right-of-way. Sidewalk cafes, including their furnishings, fencing, landscaping and awnings are permitted to encroach into the street right-of-way subject to maintaining a minimum 1.8m (6ft) unobstructed, pedestrian clearway at the sidewalk.

- Rear Yard setback:** Minimum 6.0m (19.7ft) where abutting properties containing heritage house forms. Where on a corner site flanking a heritage house form, a distance equal to the side yard setback of the existing building on the property abutting the rear property line and for a depth no greater than the depth of that building. Otherwise a rear yard setback is not required.
- Side Yard setback:** Prohibited unless associated with a publically accessible public space or pedestrian passage.

4.0 development standards: downtown core

Typical parking placement & access



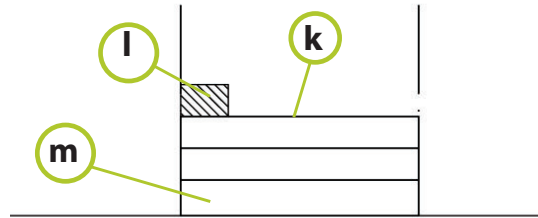
Parking Placement & Access

On-site parking is permitted below and above-grade, or to the rear where at-grade. At-grade parking shall be placed within the shaded area as indicated in the figures and subject to the following standards:

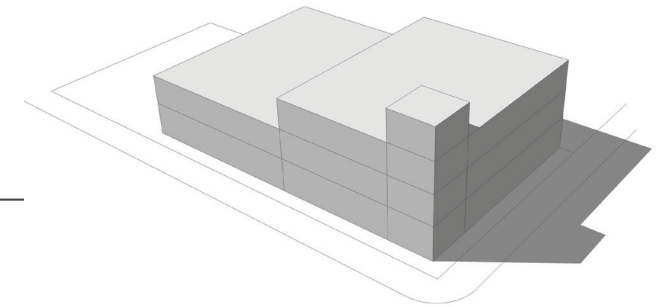
- e. **Front Yard setback:** The minimum being the distance equal to 30% of the lot depth, but no less than 15m (49.2ft).
- f. **Flank Yard setback:** Minimum 6.0m (19.7ft).
- g. **Rear Yard setback:** Not required.
- h. **Side Yard setback:** Not required.

- i. **Vehicular Access:** Front driveway access will only be permitted for properties with frontages of 50.0m (164ft) or greater. Adjacent property owners are encouraged to coordinate and consolidate vehicular access points, as only one front drive access shall be permitted on any given block. Corner lots are permitted to provide a rear driveway accessed from the side street. To minimize breaks in the street wall, vehicular access points shall be architecturally integrated within the street wall.
- j. **Above-Grade Public & Private Parking:** Parking at the grade level is subject to the above parking placement setbacks to enable commercial or institutional uses fronting onto the street. Upper parking levels to a maximum height* of 12.5m (41ft) are subject to the setbacks for the Principle Building Placement and the applicable design guidelines in Section 5.0.

Typical building profile



Typical massing



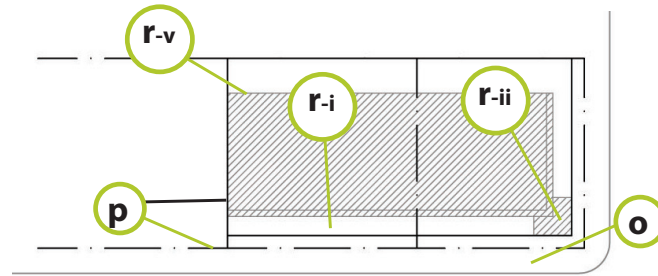
Building Height* & Massing

Structures shall comply with the following standards:

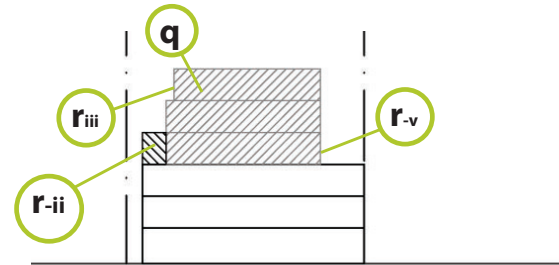
- k. **Building Height:** Minimum 2-storeys or 7.5m (24.6ft). Maximum 3-storeys or 12.5m (41ft).
- l. **Key Corner Sites:** Subject to the Design Guidelines in Section 5.0, the corner of the building for a length of 6.0m (19.7ft) along either street frontage may exceed the height limit by 1-storey or 4.0m (13.1ft) to a maximum height of 16.5m (54.1ft).
- m. **Grade Level Height:** Minimum 4.5m (14.8ft) floor-to-floor grade level height.
- n. **Massing:** Building profiles, massing and rooflines should be consistent with the character of similar building types on the surrounding blocks and subject to the applicable design guidelines contained in Section 5.0.

*Heights shall be measured from a reference plane representing the average grade level of the property

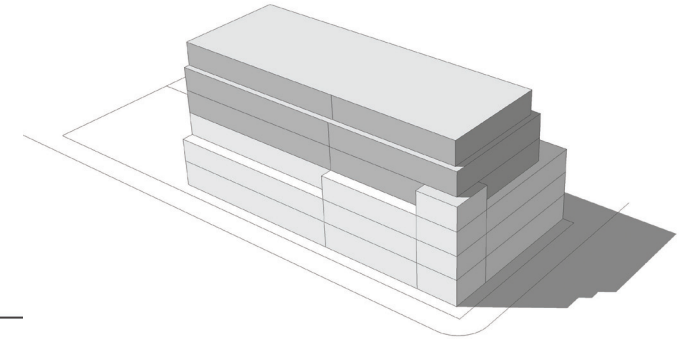
Typical bonus height placement for buildings up to 6-storeys



Typical profile for bonus height for buildings up to 6-storeys



Typical massing for bonus height for buildings up to 6-storeys



Bonus Height* & Massing

A significant number of existing and approved buildings in the Downtown Core area exceed the 3-storey height limit and include 6, 8 and one 10-storey tower. Accordingly, where appropriate and desirable, a bonus height may be considered in exchange for securing specific public benefits such as heritage retention, affordable housing, or cultural and community facilities (see Section 6.0 for recommendations on a bonus formula). Bonus heights are subject to the following development standards:

- o. **Principle Street Frontage:** Only properties with a principle frontage on either a street right-of-way or open space of 80ft or greater are eligible for a height bonus. This is to avoid a 'canyon' effect by ensuring that building heights are proportional to, or do not exceed, the width of the street or open space they frame.

- p. **Lot Dimensions:** Minimum frontage of 30.0m (98.4ft) for interior lots and 25.0m (82ft) for corner lots for building up to 6-storeys. Minimum frontage of 45.0m (148ft) for interior lots and 41.0m (135ft) for corner lots for buildings taller than 6-storeys. Minimum lot depths of 30m (98.4ft) for building up to 6-storeys and 36m (118ft) for taller buildings. This is to ensure an adequate lot size for a viable development that can be appropriately designed and massed to mitigate the additional height while providing all other necessary elements such as on-site parking and amenities.

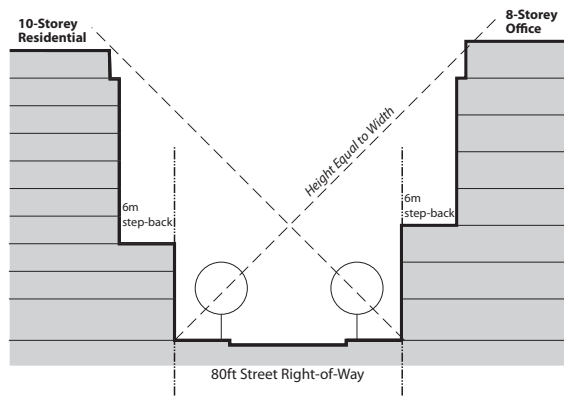
- q. **Bonus Height:** A bonus of up to 7-storeys may be considered to a maximum building height of 32.5m (106.6ft). This translates into an 8-storey office building or 10-storey residential building. This maximum height in combination with the required step-backs maintains a proportional relationship to the street, so as not to visually and physically overwhelm the pedestrian experience at the sidewalk. Additional heights would require additional step-backs from the street to maintain this balance.

- r. **Massing for buildings up to 6-storeys:** The components above the base building that are a bonus in height shall be subject to:
 - i. A minimum 3.0m (9.8ft) step-back from the building's 2 or 3-storey street wall. This is to reinforce the low-rise street wall that characterizes the Downtown Core area.
 - ii. Subject to the Design Guidelines in Section 5.0, sites identified as Key Corner Sites are not required to provide a step-back at the corner of the building for a length of 6.0m (19.7ft) along either street frontage for a maximum height of 4-storeys or 16.5m (54.1ft).
 - iii. A minimum 1.0m (3.3ft) step-back for the top storey to enhance building expression.

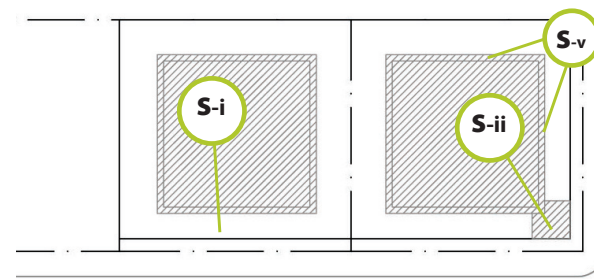
*Heights shall be measured from a reference plane representing the average grade level of the property

4.0 development standards: downtown core

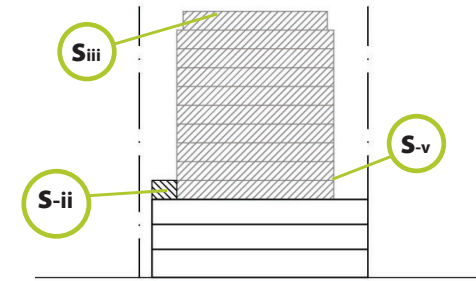
A maximum height of 32.5m (106.6ft) maintains a proportional relationship to the street



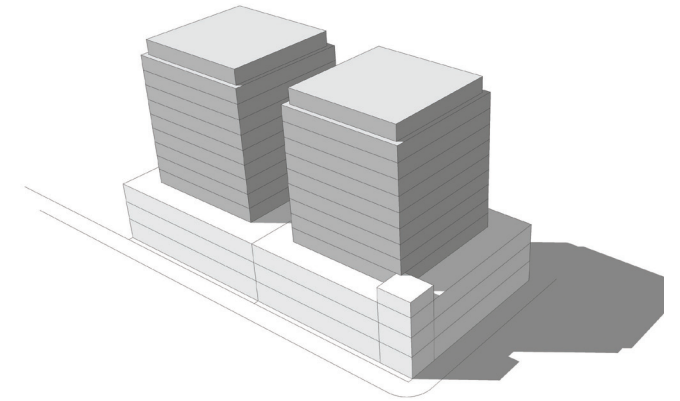
Typical bonus height placement for buildings greater than 6-storeys



Typical profile for bonus height for buildings greater than 6-storeys



Typical massing for bonus height for buildings greater than 6-storeys

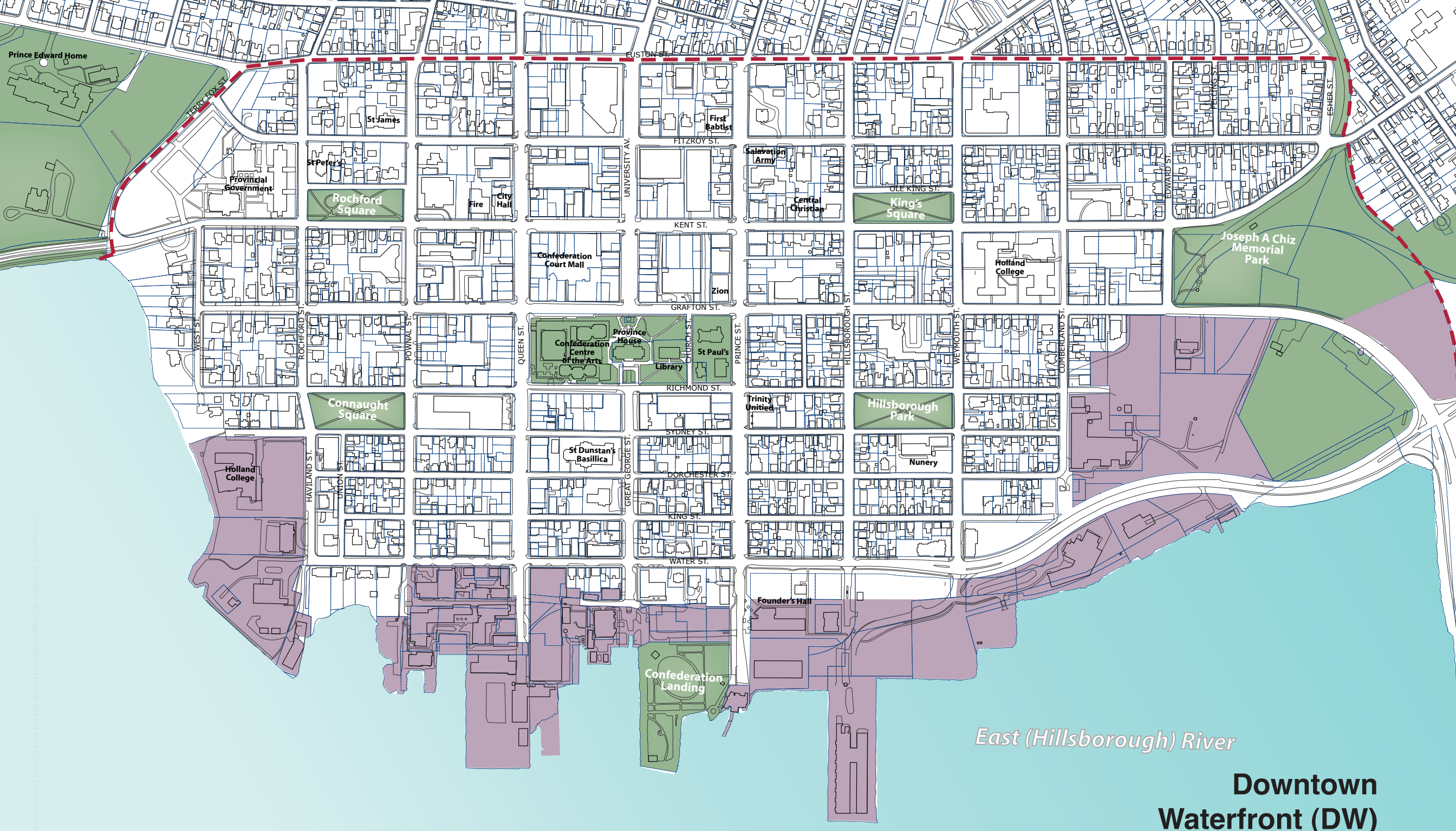


- iv. For a bonus height greater than one storey, a minimum 6.0m (19.7ft) step-back is required from any retained and integrated heritage structures on the property, in addition to being consistent with all other applicable heritage related policies and design guidelines.
- v. A minimum 5.5m (18ft) rear yard setback and 5.5m (18ft) side yard step-back where the building frontage exceeds 50.0m (164ft) so as to provide a visual break and to enable glazing on rear and sidewalls with adequate separation distances.

- s. **Massing for buildings greater than 6-storeys:** The components above the base building that are a bonus in height shall be subject to:
 - i. A minimum 6.0m (19.7ft) front yard step-back above a minimum 3-storey podium or base building. This is to reinforce the low-rise street wall that characterizes the Downtown Core area. If a building exceeds the maximum 32.5m (106.6ft) height, an additional step-back equal to the additional height will be required to maintain a proportional relationship with the street.
 - ii. Subject to the Design Guidelines in Section 5.0, sites identified as Key Corner Sites are not required to provide a step-back at the corner of the building for a length of 6.0m (19.7ft) along either street frontage for a maximum height of 4-storeys or 16.5m (54.1ft).

- iii. A minimum 1.0m (3.3ft) step-back for the top storey to enhance building expression.
- iv. A minimum 6.0m (19.7ft) step-back from any retained and integrated heritage structures on the property, in addition to being consistent with all other applicable heritage related policies and design guidelines.
- v. The massing above the podium or base building shall be in of a slender point-tower form. The tower component shall have a maximum gross floor plate size of 750sm (8,000sf), minimum 10m (33ft) interior yard setbacks, and a maximum 25.0m (82ft) tower width addressing the principle street frontage.

- t. **Landmark View Plane:** where bonus heights are considered on properties subject to the Landmark View Plane, the additional heights shall not be visible over Province House from the vantage of a pedestrian (1.7m or 5.8ft) facing north on Great George Street at any point between Richmond Street and Dorchester Street
- u. **Supporting Studies:** to be determined on a case-by-case basis, additional studies and analysis may be required to ensure that the additional height and/or its massing are meeting the desired performance standards with respect to mitigating visual, shadow, wind, traffic and heritage impacts.
- v. **Design Quality:** Developments considered for a height bonus shall demonstrate, to the satisfaction of the City, the highest possible standards in architectural, landscaping and material quality that is consistent with all applicable design guidelines in Section 5.0.



East (Hillsborough) River

Downtown Waterfront (DW)

4.5 Downtown Waterfront (DW)

Description

The Downtown Waterfront (DW) defines the large scale and evolving lands associated with the central waterfront area. These areas currently contain a variety of uses and building types. Given the scale of the lands and the lack of a well-defined organizing structure, these areas require comprehensive planning when considered for development. This is to ensure a coordinated and cohesive outcome that includes a fine-grained network of streets and blocks, open spaces and other amenities. In general, the uses should be mixed, heights should step down to the waterfront and grade-level uses should animate the water's edge in all seasons.

Development Standards

Given that development will not occur in an incremental and infill manner as elsewhere in the 500 Lot Area, a specific set of Development Standards were not defined for the Downtown Waterfront at this time. It is anticipated that as this area is comprehensively planned, development standards will emerge that may draw on and/or combine the standards of the other Urban Character Areas. The Downtown Waterfront traverses across the entire southern limits of the 500 Lot Area, abutting all the Character Areas, except for the Downtown Core. Accordingly, it is appropriate that the development standards adopted for a given area would be consistent with that of the adjacent Urban Character Area. For example, the foot of Queen Street would logically extend the development Standards for Downtown Main Street into this part of the Downtown Waterfront.

Comprehensive Development Guidelines

Similar to other maritime cities, the central waterfront area no longer serves as a working industrial port. Consequently a tremendous opportunity is afforded to transform the waterfront into a destination neighbourhood and to re-stitch it back to the 500 Lot Area.

While much of the area is currently subject to the Comprehensive Development Area zone and is largely in the control of the Charlottetown Area Development Corporation, there is currently no clearly defined vision or guiding plan for the waterfront. Despite some very positive interventions such as Confederation Landing, other developments have been subject to some controversy. Stakeholders have expressed enthusiasm about the prospects for the transformation of the waterfront but are greatly concerned about uncertainty of how it will unfold given recent developments and a perceived lack of a guiding and coordinated plan. Visitors that arrive by cruise ships that were interviewed also perceived the waterfront at fragmented and disorienting.

Given the tremendous and precious asset that the Downtown Waterfront presents to the city as a whole, it should be carefully planned in a comprehensive and transparent manner. Much can be learned from other cities that have transformed their waterfront areas. Accordingly, the following comprehensive development guidelines are informed by best practices and tailored specifically to the Charlottetown context.

Uses

- Plan for a dense and fine-grained mix of uses to ensure a critical mass of activities and people to animate the waterfront in all seasons and hours
- Ensure commercial grade level uses that engage and animate the pedestrian environment and water's edge
- Provide for a series of anchor attractions across the waterfront to stimulate pedestrian circulation

Built Form

- Plan for a compact, human-scaled built environment that can provide a comfortable sense of built continuity and enclosure
- Provide for a variety of building typologies that are compatible with adjacent Urban Character Areas
- Step down height to the water's edge and to adjacent low-rise areas
- As the 'front door' to the Downtown, buildings on the waterfront shape the image of the City and are the first and last impression to visitors arriving by water – accordingly the highest architectural quality is critical consideration
- New developments should strive to showcase sustainable design as well as measures to address anticipated sea-level rise

Circulation

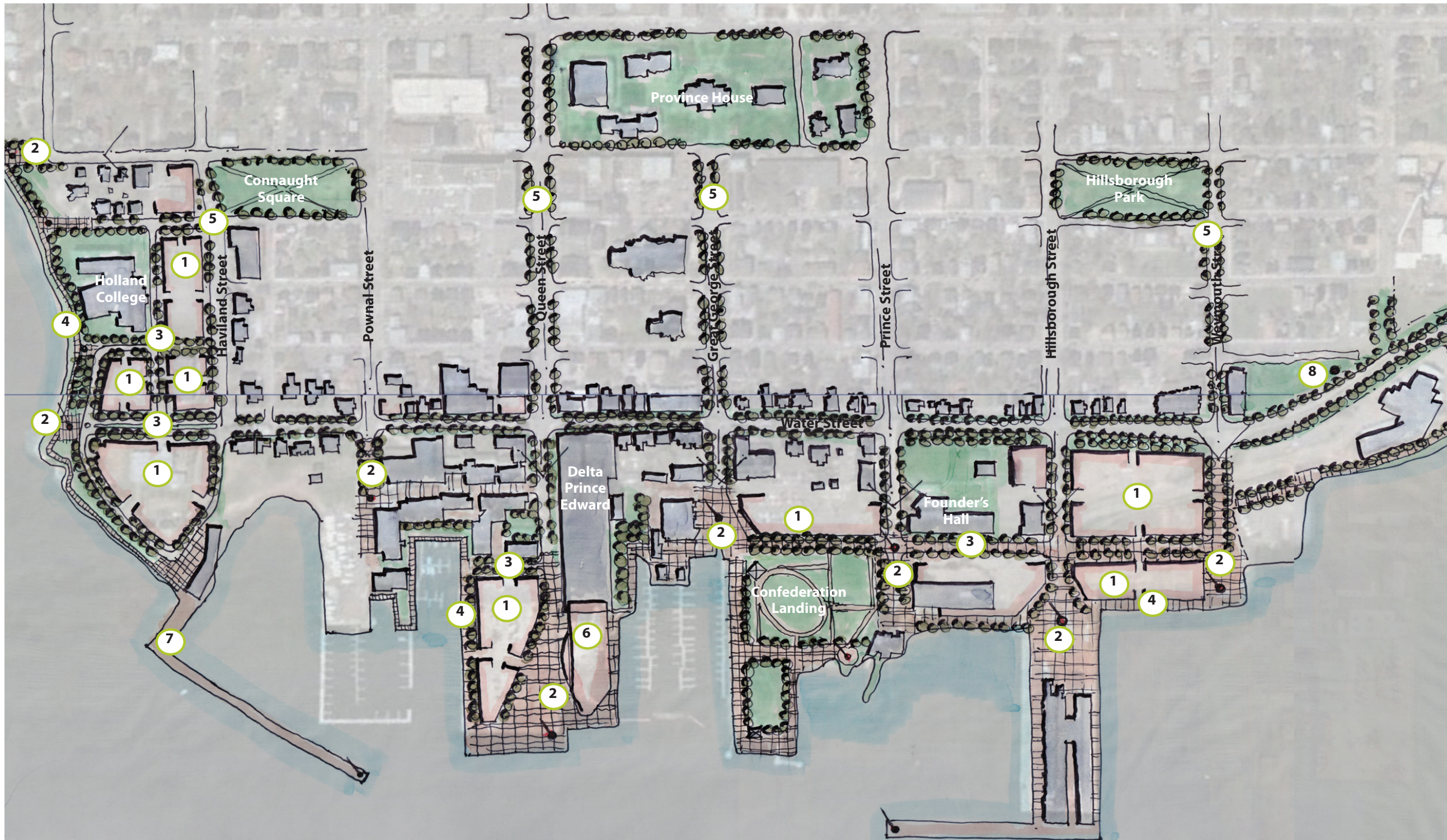
- Seamlessly integrate into the Downtown by extending the historic and walkable street and block pattern into the waterfront area
- Waterfront streets should be designed for pedestrian-priority with distinctive paving and other traffic calming measures
- Surface parking areas should be concealed as much as possible, otherwise designed as multiuse spaces that can be utilized for special events

Views & Connections

- Ensure continuous and unfettered public access to and along the waterfront
- All end of street views to the waterfront should be protected and reinforced with public art or landmark pavilions
- Wayfinding should be an integral part of the design of the public realm

Open Spaces

- Provide a variety of open space scales, types and programming to appeal to broad demographic and recreational interests from passive to active
- Ensure that open spaces are safe and animated by ensuring they are adequately framed by buildings with active grade-level uses



4.0 development standards : downtown waterfront



- 1 Mixed-use development blocks of similar scale and character to the rest of the 500 Lot Area
- 2 End of street views to the waterfront protected and enhanced as a series of public spaces featuring landmark public art installations
- 3 A fine-grained network of pedestrian priority cross-streets to enhance connectivity
- 4 Continuous public waterfront access provided through a linked boardwalks, promenades, and recreation trails
- 5 Streetscape and wayfinding enhancements along key links between the waterfront and other major downtown attractions and open spaces
- 6 A new convention centre designed as a signature architectural landmark and waterfront attraction
- 7 Another waterfront attraction in the form of a new pier that also serves as a breakwater for an enhance marina facility
- 8 A new open space and public art installation to serve as an eastern gateway to the waterfront and downtown

Demonstration Waterfront Concept Plan

Prepared during the workshop, this plan is a concept that demonstrates one scenario of the build-out of the Downtown Waterfront in a manner that would be consistent with the Comprehensive Development Guidelines.





5.0 design guidelines

The following *Design Guidelines* provide direction for the character and quality of new infill development in the 500 Lot Area. These guidelines complement the *Development Standards* of the previous section, providing more qualitative considerations as well as greater flexibility in meeting design objectives. The primary intent is to reinforce the best qualities and characteristics of the different character areas.

Although these guidelines focus on providing direction and benchmarks for redevelopment and infill within, and with regard to, a heritage context, these are not detailed guidelines that deal specifically with the preservation and restoration of heritage resources. In that regard, there are a number of existing adequate resources that could be read in conjunction with these guidelines, including:

- *Design Guidelines for the Preservation of Historic Resources, City of Charlottetown*
- *Guidelines for the Repair and Replacement of Historic Windows, City of Charlottetown*
- *Signage Guidelines, City of Charlottetown*
- *Design Guidelines, Downtown Charlottetown 500 Lot Strategy, Ekistics, 2006*
- *Standards and Guidelines for the Conservation of Historic Places in Canada, Government of Canada*

The Design Guidelines are organized according to the following sections:

- *General Design Guidelines for Downtown Neighbourhood & Mixed-Use Neighbourhood Areas*
- *General Design Guidelines for Downtown Main Street and Core Areas*
- *Design Guidelines for Development On or Adjacent to Heritage Properties*

Design Principles for New Development in a Heritage Context

A fundamental objective of these Standards & Guidelines is protecting and conserving the structures and landscapes that have significant heritage value, and, managing the introduction of new structures and landscapes in such a way that they contribute to, and enhance, the distinct 'sense of place' of the 500 Lot Area.

As is has occurred historically, new architectural styles will be introduced and invariably be constructed within, and adjacent to, heritage properties. The objective for the design of new buildings should not be to replicate or mimic historic buildings. Rather, the objective should be to create a respectful and well-designed new structure "of its time" that fits and is compatible with the character of its immediate context.

Subject to all other sections of these Standards & Guidelines, key design principles for new development within this historic context, include:

Contemporary Design - New design should not seek to overtly contrast with its surroundings; rather, it can be well mannered and respectfully fitting in its context while being a distinctive design expression. Drawing on the past can be appropriate, however, it should avoid confusing or blurring the line between what is and isn't authentic heritage or an architectural style.

Scale & Proportions - Architectural composition has always had at its root the study of proportion. In the design of new buildings in a heritage context, the proportions of buildings in the immediate context should inform what scale and proportions constitute a good fit.

Street Interface - The streets and their varying characteristics in the Lot 500 Area not only organize and connect the area; they also shape how it is experienced. A key aspect of their success is in how buildings have historically interfaced and defined the character of the streets. Above all things, new buildings must build on this tradition with respect to design that properly addresses and animates the public realm.

Fenestration & Detailing - For new buildings, detailing should refer to the heritage attributes of the immediate context. Detailing can be more contemporary yet with a reference to proportions, rhythms, opacity and transparency that is consistent with the immediate context.

Material Pallet - Whereas there is a very broad range of materials in today's design pallet, materials proposed for new buildings in the 500 Lot Area should draw from historically examples that demonstrate high quality and permanence. The use and placement of these materials in a contemporary composition can be critical to the success of how well a new building fits in its context.

5.1 General Design Guidelines for Downtown Neighbourhood & Mixed-Use Neighbourhood Areas

5.1.1 Siting & Orientation

In the Downtown Neighbourhoods the pattern of house setbacks and orientation is set primarily by the historic grid of streets as well relationship of residential units within the neighbourhood. New or altered homes should reflect the siting of adjacent residences. On the narrow east/west streets, homes are generally set at the property line. On many north/south streets there is a generally a greater setback from the street.

- Building fronts shall be in line with traditional houses along the block.
- When constructing a new building or addition, locate it to fit with the predominant pattern of yard dimensions seen on the block.
- Where setbacks vary, a new building shall fit within the range of setbacks seen traditionally in the block.



- A majority of the area of the front setback shall be ground cover or low planting.
- All structures should have one primary entry that faces the street.
- Additional entrances may be located to the side or rear.
- Multi-unit structures should be street oriented.
- Orient the stoop or front porch to the street or both streets if on a corner



5.1.2 Parking Access & Garages

There are few examples of garages that access directly from the street leading to the high quality of the streetscape and pedestrian experience. This quality should be continued for any modification, additions or infill. Generally all parking that is located on the same lot as the home will be located at the rear of the building and access from the street or a lane where they exist.

- parking spaces should be located in the rear half of a building lot.
- Parking should not be located in the front setback and should not extend beyond the front plane of the primary building.
- Minimize the visual impact of a garage by locating it to the rear of a building lot,
- Detached garages are preferred.
- Shared driveways and curb cuts are encouraged, both with adjacent properties and for multi-unit housing.
- For row houses and multi-unit structures parking in a interior courtyard with a single access point is preferred to multiple driveways.
- Curb cuts should accommodate single-car access and single-car widths should be maintained until the driveway extends beyond the rear of the primary structure.

5.1.3 Mass & Scale

The mass and scale of a new building is an important design issue for the neighborhood areas of the 500 Lot Area. To the greatest extent possible, new construction should maintain the scale of the surrounding homes. While new buildings may be larger than many of the early houses, the new construction should not be so large that the visual continuity of the neighborhood is compromised.

- The primary plane of the front should not appear taller than those of typical residential structures in the neighborhood.
- The back side of a building may be taller than the front and still appear in scale if appropriately designed and compatible with the primary structure.
- A new multi-unit structure (where permitted) should not overwhelm existing single-family structures.

- A single wall plane should not exceed the maximum façade width of a typical residence. If a building is wider overall than those seen typically, divide the large façade into subordinate wall planes that have dimensions similar to those of traditional buildings on the block.
- Building forms should be consistent with traditional homes, typically simple rectangular forms



5.1.4 Roofs

In general roofs on any infill buildings in the neighborhoods should reflect that of adjacent or nearby residences. The common characteristic of roofs within the 500 Lot Area is a relatively steep pitch and often with dormers.

- Front facing roofs should include accent gables, dormers, and a variation of rooflines to lend variation
- The soffit should have a consistent overhang with that of adjacent buildings;
- All stacks, gas flues, and roof vents should not be visible from the front or from side elevations in the case of corner lots;
- Gas flues should be located near the roof ridge to reduce their height;
- All metal chimneys should be boxed-in and finished with cladding;



- Skylights shall be located on roofs not visible from the street and should have a flat profile.
- While flat roofs are acceptable, “exotic” roof forms that would detract from the visual continuity of the streetscape are discouraged.



5.1.5 Porches & Entries

As an extension of the house, porches and other entry features establish the necessary linkage between the public and private realm of the street. They are common on certain historic styles such as Queen Ann styles.

- Porches should only be incorporated in house styles that typically include them
- Porches on single-detached units shall be deep enough to allow a seating area of a minimum depth of 1.5m
- The height of the porch should match that of nearby homes of the same historic style, and shall be constructed of materials used elsewhere on the home
- Porch steps should be detailed in the same material as the porch floor.



5.1.6 Windows

The proper placement and coordination of windows and doors is essential to creating a pleasing facade and should sensibly relate the interior space to the street.

- Windows should occur on the front facade of all housing types
- Windows should reflect that of the nearby homes or the style that is reflected in the design. Very often the historic precedents in the 500 Lot Area reflect a higher degree of solid to glazing than in contemporary designs.
- Where possible, a window, sidelights, or transom should accompany the front door
- Windows on all facades should vertically and horizontally align with each other and other features of the house and those of adjacent homes



- Architectural details should be implemented to emphasize doors and windows
- Sliding doors are not permitted on the front or flankage of the building where visible from the street



5.1.7 Corner Sites

Corner lots are those lots that occur immediately adjacent two intersecting streets. Homes on corner lots have two options for locating a front entrance. The following guidelines make full use of the opportunities inherent in a corner lot situation:

- The front, exterior side, and rear elevations of corner lots should carry a consistent type and quality of materials and window treatments
- Architectural features such as a porch, turret, or bay window should wrap the corner of the house and address the corner condition
- Decorative fences may enclose the flankage and front yards to reinforce the corner of local streets



5.1.8 Materials & Finishes

New developments should be mindful of ensuring excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of the Design Guidelines is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

- The 500 lot area has a rich history that is reflected in the Georgian and Queen Ann buildings that are constructed in a variety of materials. New developments should seek to contribute to this mix and variety.
- Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance.

- Building materials recommended for new construction include brick, stone, wood and glass.
- In general, the appearance of building materials should be true to their nature and should not mimic other materials.
- Vinyl siding should be discouraged where directly fronting and visible to a public street or open space. plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be discouraged.
- Materials such as plastic, plywood, concrete block, darkly tinted and mirrored glass and metal siding utilizing exposed fasteners should be strongly discouraged.



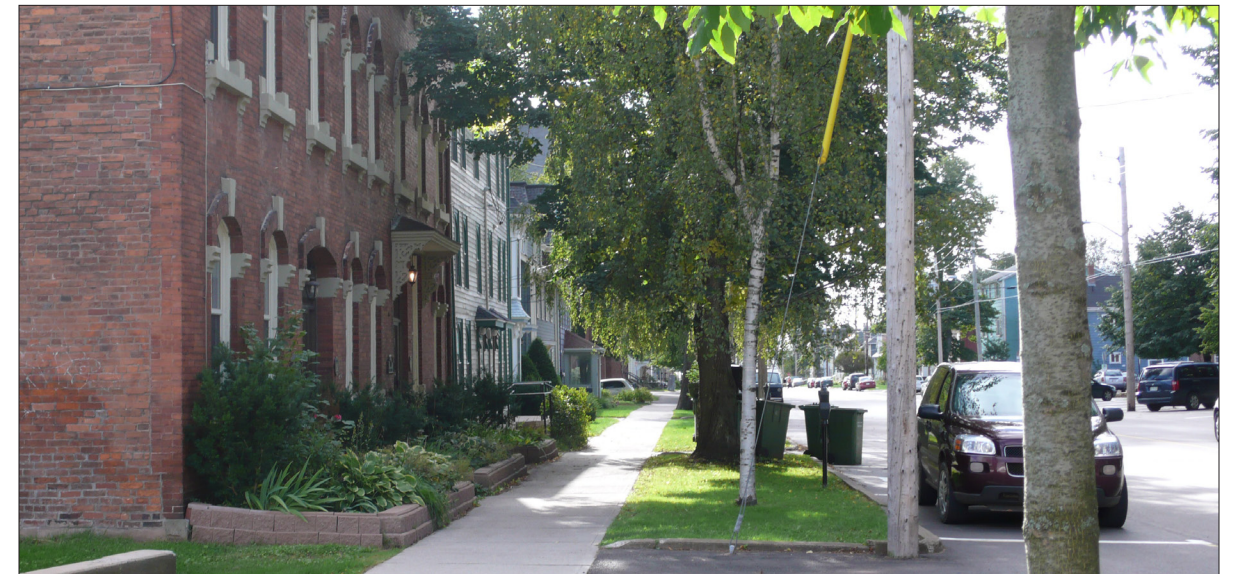
5.1.9 Landscaping

The design of streetscapes is intended to enhance the downtown experience and nurture a culture of walking. Streetscapes strengthen precinct and heritage area identities and reinforce visual and physical connections across the downtown and to adjacent neighbourhoods.

Where the right of way widths in the downtown neighbourhoods tend to be narrow, space must be used carefully. As the primacy of pedestrians is fundamental to the future of downtown, sidewalk widths should be maximized, and lane widths and vehicular space should be minimized. Street trees should be provided on all streets wherever possible. However, this may not be feasible due to space constraints. Where trees are not possible, special attention to vertical elements (lighting, bollards, etc.) should be given through regular alignment

and frequent spacing, in order to help define the pedestrian zone.

In many of the downtown neighbourhoods, residential entrances are at the street and there is little space for landscape materials. Every opportunity to provide a landscaped transition between the sidewalk and the face of the building should be utilized. Low shrubs and street trees should be part of the treatment of the façade of the building and, where possible, a consistent street treatment should be carried out along the entire block.



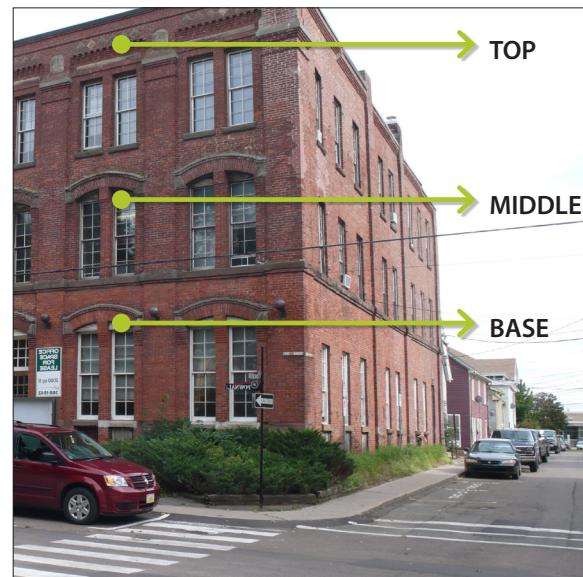
5.2 General Design Guidelines for Downtown Main Street and Core Areas

5.2.1 Building Expression

In the façade, buildings should be designed to reinforce the following key elements through the use of setbacks, extrusions, textures and materials:

- **Base** - A base should be clearly defined that positively contributes to the quality of the pedestrian environment in the level of animation, transparency, articulation and material quality.
- **Middle** – The middle or body of the building should contribute to the physical and visual quality of the overall streetscape.
- **Top** – The roof condition, expressed as an upper storey or roof feature, should be distinguished from the rest of the building and designed to contribute to the visual quality of the streetscape.

Buildings should seek to contribute to the mix and variety of high quality architecture. The articulation of building mass through vertical and horizontal recesses or projections, datum lines, and changes in materials, texture or colour should be encouraged.



5.2.2 Building Orientation & Placement

The orientation and placement of buildings along the street help to reinforce the public realm by enhancing the pedestrian environment through creating a sense of enclosure. This is achieved by framing the street with parallel aligned buildings, providing the appropriate levels of animation and use. Key guidelines for the orientation and placement of buildings are as follows:

- All buildings should orient to and address the street with clearly defined entry points that directly access the sidewalk.
- Buildings should be placed at or close to the street edge, consistent with adjacent buildings.
- Development of an entire block or at corner sites may provide greater setbacks to widen sidewalks without compromising the visual continuity of the streetscape.



5.2.3 Grade Level Storefronts

Well proportioned and designed storefronts can contribute positively to the pedestrian environment by providing animation and visual interest at the sidewalk. New building storefronts should incorporate the following principles:

- To reflect the existing character and context, storefronts should generally have a frontage in the range of 7.5 metres (25ft) or less, but not greater than 15 metres (49ft).
- Where retail frontages are greater than 7.5 metres (25ft), the appearance of narrow storefronts should be articulated in the design of the facade.
- Storefronts should have a high-level of transparency, within a range of 75% glazing to maximize display areas, as well as visual interest and animation.



- Clear glass should be used for wall openings (e.g., windows and doors) along the street-level façade. Dark tinted, reflective or opaque glazing should be discouraged for storefronts.
- An identifiable break or gap could be provided between the street-level uses and the upper floors of a building. This break or gap may consist of a change in material, change in fenestration, or similar means. The identifiable gap or break can emphasize the storefront while adding visual interest and variety to the streetscape.
- Storefront entrances should be highly visible and clearly articulated. Entrances should be located at or near grade. Split level, raised or sunken entrances are strongly discouraged.
- Weather protection for pedestrians is encouraged through the use of awnings and canopies.



5.2.4 Corner Sites

Corner buildings have a greater visual prominence given that they front onto two streets and frame intersections. Accordingly, they have a greater civic obligation to should be designed to give good form and address to the corners they occupy.

- To enhance the distinction of new buildings at key corner sites, modest exceptions to setbacks and height restrictions should be permitted to encourage massing and designs that accentuate the visual prominence of the site – architectural treatments can include tall slender elements such as spires and turrets.
- New developments on all corner sites should orient to both street frontages with entrances.
- As new developments on corner sites can shape the image and character of an area, the highest possible standards in design and material quality should be encouraged.



5.2.5 Street Access Residential

All uses should help create an animated street environment with doors, windows and pedestrian activity fronting and accessing directly onto the sidewalk. Where retail is not required, and residential uses are proposed at-grade, the following guidelines apply:

- Residential uses at-grade should include individual units accessed from the street, designed to accommodate conversion into live-work or retail uses.
- Appropriate front yard privacy measures should be considered such as setbacks, landscaping, and porches.
- Access to the individual units should be clearly visible, and the scale, rhythm and articulation of the street wall should be consistent with the residential character of adjacent neighbourhoods.



5.2.6 Roof Treatment

The design of the roof can make an impact on the character of the streetscape, especially from great distances. Roofs of buildings are also seen from other buildings of equal or greater height. Roof design should consider the following guidelines:

- The expression of the building top and roof, should be clearly distinguished from the rest of the building through treatments such as stepbacks, change in materials, cornices lines, and overhangs.
- Mechanical penthouses should be integrated with the architectural treatment of roofs and/or screened from view.
- Green roofs should be encouraged to provide for aesthetic as well as functional and sustainable considerations.



5.2.7 Surface Parking Areas

Surface parking areas should be considered an extension of the public realm when they are visible from the street. Where possible, parking lots should be located behind buildings and screened from the street.

Where parking lots are visible, a coordinated landscape/fence edge should be created between the sidewalk and the parking lot. Consideration should be given to breaking surface lots down into smaller parking “pods” through the use of trees and lighting. Pathways between cars and the public sidewalk or building entrances should be clearly defined through the use of paving materials, landscaping and lighting.



5.2.8 Parking Structures

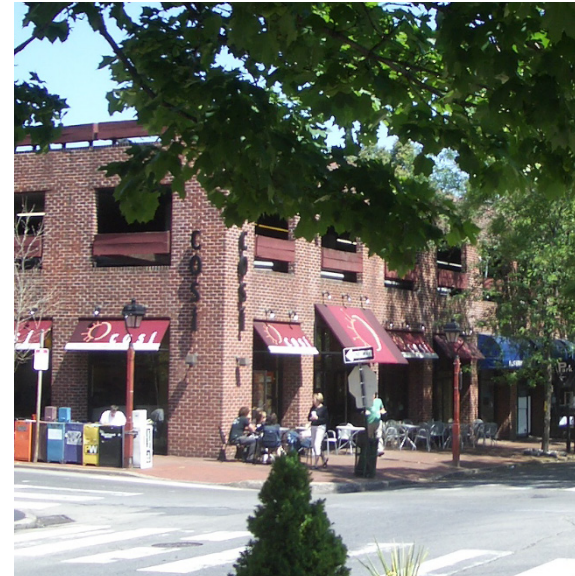
Wherever possible, parking for new developments should be provided at the rear of the building. Where parking is provided above-grade within the base building and if structured parking is contemplated for the 500 Lot Area in the future, the following guidelines address the design and quality of such structures.

- Direct access for parking from the primary street should be discouraged.
- Where an above-grade parking facility fronts on a street, the ground-level frontage should incorporate retail, public or other active uses, as well as provide pedestrian amenities such as awnings, canopies, and sheltered entries.
- Above-grade parking structures should be designed in such a way that they reinforce the local built character and blend into the streetscape through facade treatments that



conceals the parking levels, utilize high quality materials, and gives the visual appearance of a multi-storey building articulated with bays and ‘window’ openings.

- Stairways, elevators and entries should be clearly visible, well lit and easily accessible.
- Signage and wayfinding should be integrated into the design of public parking structures. Integrating public art and the lighting of architectural features should also be considered. This will reinforce its unique identity, and aid visitors in finding them upon arrival.
- The impact of interior garage lighting on adjacent residential units should be minimized, while ensuring that safe and adequate lighting levels are maintained.



5.2.9 Materials & Architectural Quality

New developments should be mindful of ensuring excellence in architectural design and in the use of high-grade materials, particularly at street-level. A key objective of the Design Guidelines is to achieve a balance between consistencies in design quality and street interface, while enabling individual expression in new developments. Key guidelines for architectural and material quality include:

- Building materials should be chosen for their functional and aesthetic quality and exterior finishes should exhibit quality of workmanship, longevity, sustainability and ease of maintenance.
- Building materials recommended for new construction include brick, stone, wood, glass, in-situ concrete and pre-cast concrete.
- In general, the appearance of building materials should be true to their nature and should not mimic other materials.
- Mirrored glass and metal siding utilizing exposed fasteners should be discouraged.

5.2.10 Signage

Signage plays an important role in the overall image of any area. Signs should contribute to the quality of individual buildings and the overall streetscape. They should reflect the unique characteristic of their context. This includes compatibility with heritage buildings, where appropriate. High quality, imaginative, and innovative signs are also encouraged. Design objectives for commercial storefront signage include:

- Commercial signage should not overwhelm the building and/or the storefront.
- Back lit illuminated rectangular sign boxes are discouraged.
- To minimize visual clutter, signage should be integrated into the design of building façades wherever possible, through placement within architectural bays and friezes.

- Signage should not obscure windows, cornices or other architectural elements.
- Large freestanding signs (such as pylons), roof signs, and large-scale advertising (such as billboards) should be discouraged.
- The maximum signage area for storefront signs should be 0.1 square metre (1sf) for every linear 0.1 metre (1ft) of storefront, or 25% of the storefront area - which ever is less.
- Signage on heritage buildings should be consistent with traditional sign placement such as on a sign band, window lettering, or within the existing architectural orders.
- Signage should aid pedestrians and drivers in navigating the area, especially at night.
- Signs should be well maintained and constructed using high quality materials.



5.2.11 Sustainable Design

Sustainable design can be defined as architecture and engineering that establishes the conservation of natural resources and systems as a primary consideration in the planning, design, and construction process. To achieve this goal, proposed projects are encouraged to employ sustainable design methods that strive to achieve standards set by LEED, Energy Efficiency Building Code, R-2000, Green Globes, Energy Star or other equivalents.

Sustainable design guidelines for buildings:

- Renovate and Adaptively Reuse Existing Heritage or Other Buildings - meet the Building Code for heating, ventilation, air tightness and glazing.
- Reuse of Building Materials and Recycled Materials – locally produced and sold products; project materials comprised of 5% reused and 8% recycled content.
- Energy & Water Efficient Fixtures and Appliances – energy efficiency in 80% of fixtures and 70% of appliances; use CFL Light fixtures; and, install water efficient fixtures.
- Green Energy - provide on site renewable energy to self-supply 5-10% of energy needs; and, purchase 10- 25% of energy needs through grid source or renewable energy.
- Building Orientation - Use a high performance building shell; orient and design with windows that provide 50-75% of internal lighting with natural light and take advantage of passive solar heating and shading; use no more than 30% glazing and utilize higher standards for frames and glazing.

Sustainable guidelines for sites considerations:

- Land Usage – locate in existing urban area within walking distance of jobs, housing, service centers, schools, and transit; use less land at higher densities and mixed uses.
- Storm Water Runoff and Retention - use pervious pavement system, stormwater retention ponds, and green roofs to capture at least 50% of rain water to be treated and reused for irrigation of landscaping, flushing or cleaning vehicles.
- Urban Forests - retain all trees and plant large native shade trees and shrubs to achieve a canopy with 20% coverage at maturity; maintain trees with a minimum of 30sm of high quality soil and provide a non-potable water system; and, use drought resistant plants.
- Light Pollution - avoid directly lighting the sky and adhere to bird friendly development guidelines.
- Construction Activity - minimizing air and dust emissions during demolition, retrofits and refinishing.
- Urban Heat Islands - provide cover including landscaping that shades up to 30% of hard surface parking, walkways and roofs; and, cover 50% of the roof with light coloured or reflective materials.
- Linkages – support active transportation through linkages to open spaces, walking and bicycle trails, and transit; provide a space for transit shelters; and, provide bicycle storage and showers for cyclists.

5.3 Design Guidelines for Development On or Adjacent to Heritage Properties

These guidelines inform the design of new structures on or adjacent to properties of heritage significance in the 500 Lot Area. While the underlying principles and objectives are also applicable to the Neighbourhood areas, these guidelines focus primarily on the Main Street and Core urban character areas, where development pressures, the scale and magnitude of proposals, and the sense of threat to heritage resources is most significant.

The *Standards & Guidelines* seek to create a framework and strategy where by significant growth and development and meaningful heritage protection are occurring concurrent and as much as possible in conjunction with one another. The success of downtown Charlottetown depends equally on the coexistence and interplay of both: the rich appeal and distinction of 'place' that heritage and character retention affords; and, the economic and cultural vibrancy that a critical mass of people, activity and investment can deliver.

There are four types of development contexts that may impact heritage, for which the guidelines address:

Infill – includes modest vacant or underutilized sites located in-between heritage properties.

Additions – includes existing heritage properties, to which modest new construction is added. This will often occur on top of existing buildings, but can also be located to the sides and/or rear.

Integrated Developments – includes sites where one or more heritage buildings are part of a larger development proposal and are wholly or partially (façade retained) integrated into a new building or grouping of buildings.

Adjacent Developments – sites that share a lot line with heritage properties, form part of a cluster of heritage buildings, or form part of a contiguous heritage street wall.

The appropriate degree and design of alterations to any building - designated or otherwise - in the 500 Lot Area should be determined and assessed according to the level of heritage significance assigned to the property. The City has assigned heritage ratings to all properties according to a grading, for which the following degree of alterations should be permitted where development through additions or integration is proposed:

Grade 1 - the entire interior and exterior of a building deemed to have heritage value on the property should be preserved and restored

Grade 2 - the entire exterior of a building deemed to have heritage value on the property should be preserved and restored, and as much as possible the interior

Grade 3 - heritage defining aspects of the exterior of a building deemed to have heritage value on the property should be preserved and restored

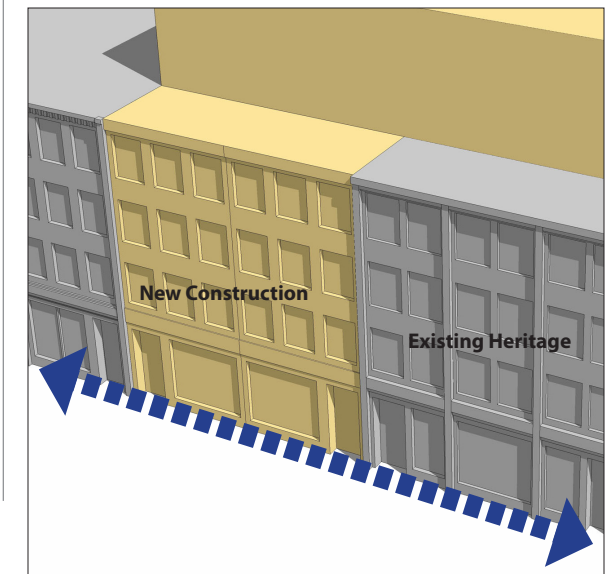
5.3.1 General Principles & Guidelines

- Design of buildings should reflect good urban design principles, the specific function and role of their particular context, and be consistent with these Standards & Guidelines.
- Some of these guidelines are by nature a subjective interpretation of qualitative factors. Subject to meeting the spirit and intent of these guidelines, creative solutions not identified in this document could be considered.
- New construction should not destroy historic materials, features, and spatial relationships that characterize a property or area.
- The new construction should be visibly differentiated from the old, achieving compatibility primarily through harmonious scale, massing, façade articulation and materiality.
- Mimicry and designs that borrow and mix historic stylistic detailing inappropriately or incorrectly should be avoided. Cotemporary design that positively contributes to area character is preferred, but where historical styles are proposed, they should be relevant to the 500 Lot Area and designed in accordance with the established orders and details of that architectural style.

5.3.2 Building Setbacks

A setback refers to the street edge grade level placement of the new construction in relation to the heritage building.

- Infill or adjacent developments should provide setbacks that are consistent with adjacent heritage buildings.
- Additions or integrated developments with street frontage may provide modest setbacks as a means of distinguishing or strengthening the prominence of the heritage building.



5.3.3 Cornice Line & Upper Level Step-backs

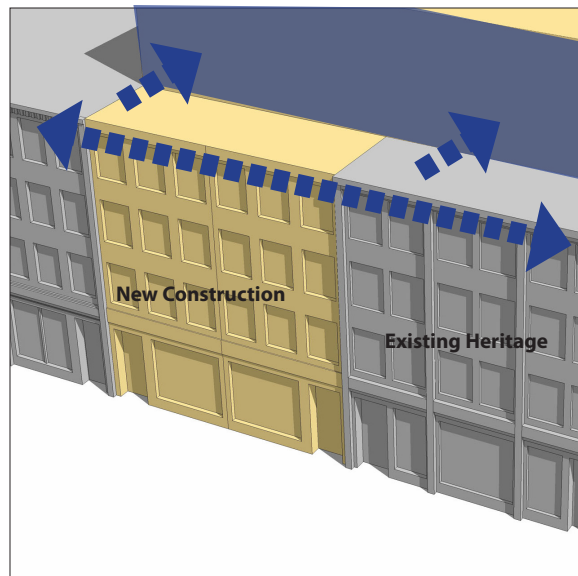
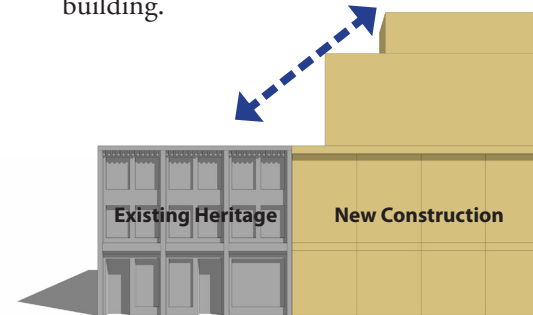
The cornice on a heritage building is the topmost element of the street wall typically detailed with a decorative moulding. The cornice line is the extended horizontal definition of the building that demarcates where the façade ends and the roof begins. When adjacent buildings have a consistent cornice line they help to reinforce the continuity of the street wall — particularly along streetscape of heritage character such as Queen Street.

A step-back refers to the setting back of the levels of a building above street wall or building base. Step-backs are useful for distinguishing new construction from heritage in additions or integrated developments, so as to maintain a consistent street wall height and to minimize the visual impact of taller elements.

- Maintain the same or similar cornice height for the base of the new building to create a consistent and complementary street wall height.
- Where infilling between heritage buildings of varying cornice heights, select the height of one to be consistent with. Where the infill site has sufficient frontage to articulate two or more bays of comparable width to the heritage buildings, the new cornice may vary its height to reference both heritage facades.
- For infill or integrated developments, step-back building elements that are taller than the street wall height of the heritage buildings a minimum of 3.0m (10ft) if only one storey, otherwise 6.0m (20ft) for two or more storeys. Where the heritage building has distinctive architectural features on its roof, step-backs of

no less than 6.0m (20ft) should be provided around those features.

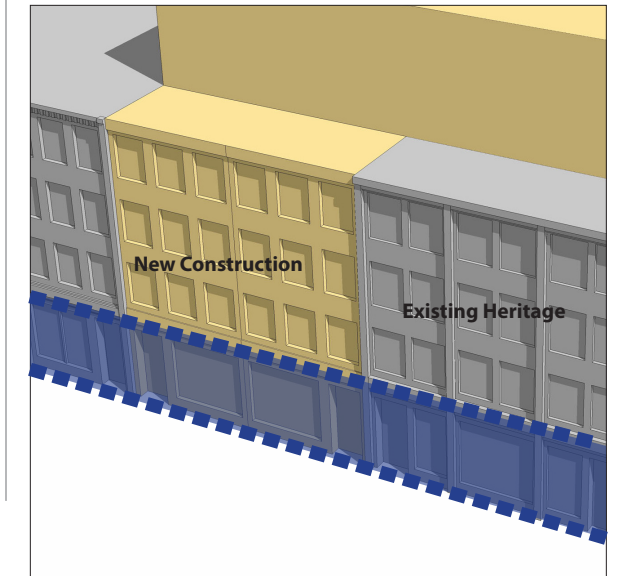
- Greater contrast in the articulation and material quality of upper levels is encouraged to further distinguish the new construction from the heritage
- For adjacent developments that are taller than the heritage building, they should visibly step down to a height consistent with the heritage building.



5.3.4 Grade level Height & Articulation

The grade level of a building provides the greatest presence on the street. Heritage buildings in particular are often characterized by their prominent and even grand grade levels.

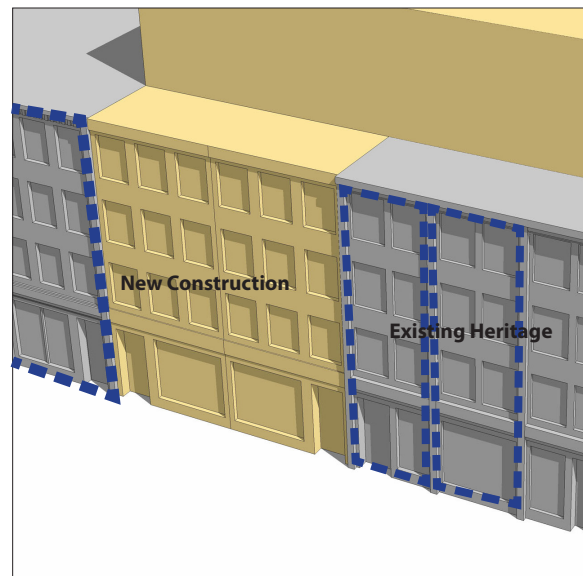
- Provide a first storey height for new buildings that is consistent with the heritage buildings. Where infilling between heritage buildings of varying grade-level heights, the taller height will set the datum for the new building.
- Maintain other heights and proportions in the first storey such as:
 - Sign band height and size
 - Window height, size and proportion, including transoms
 - Door height, position, and setback
- Maintain the prevailing at-grade use (i.e. retail or residential) but considering the intended use and role of the street



5.3.5 Proportion & Rhythm

The idea of rhythm on the façade of a building or along a street refers to the regular intervals of design elements that help to structure visual character and definition at the scale of the pedestrian. This rhythm can be expressed either at the scale of the bays of a façade, shops or at the scale of the building frontage.

- Reinforce the prevailing rhythm of the heritage buildings, generally of a narrow vertical proportion.
- For larger or longer buildings, clearly articulate vertical divisions or bays in the façade at a rhythm consistent with the heritage buildings.
- For grade-level commercial uses, the retail bays or shop fronts should be expressed in a similar scale and rhythm to adjacent heritage buildings.



5.3.6 Façade Articulation & Materiality

For infill or integrated developments, there are two alternative approaches to façade articulation:

Similarity

- Maintain the same architectural order and rhythm of the windows, and horizontal and vertical divisions of the heritage façade in the articulation of the new building
- Provide similar materials to existing heritage buildings, generally stone, brick and wood.
- Where materials differ, such as concrete, provide similar articulation on the surface through score lines or the alignment of seams of modular units.
- Provide similar colour palettes, typically neutrals and earth tones.



Complementing Contrast

- Provide a different but proportional interpretation of the existing architectural order and rhythm of both horizontal and vertical divisions of the heritage façade in the articulation of the new building.
- Provide contrasting materials and surface treatments that complement the heritage building by serving as a muted visual 'backdrop'. For example the extensive use of glass in a clean-lined modern manner can be effective for minimal visual distraction and for its transparency.
- Ensure materials and detailing is of the highest quality.



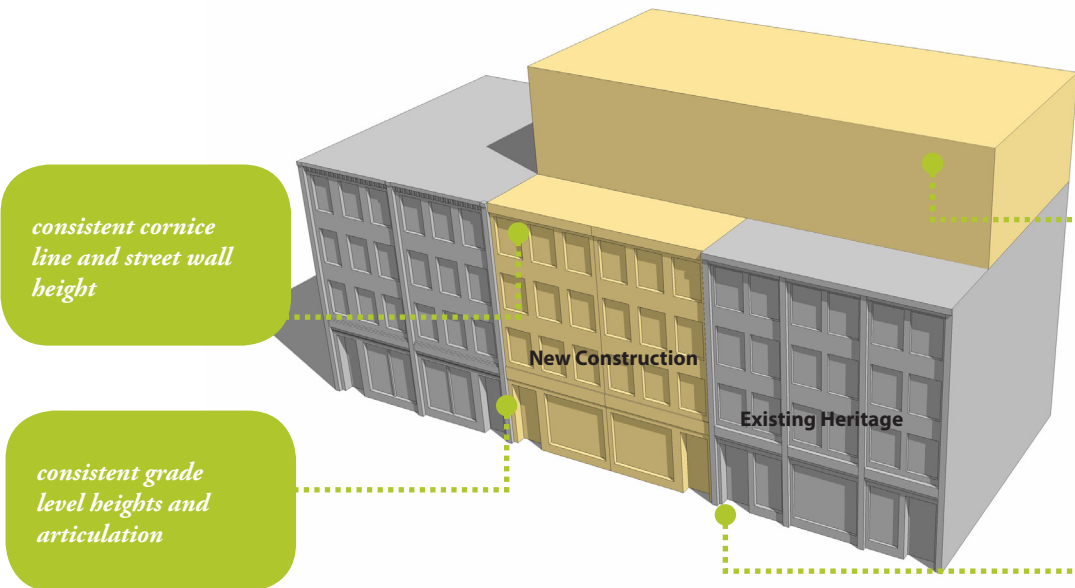
5.3.7 Façade Retention & Integration

Where only a heritage façade is retained, the façade should not visually appear to be tacked on to a new building; rather, it should be design integrated with the new construction in such a manner that the former building in its entirety visually appears to have been retained. To do so, the following guidelines apply to retained and integrated facades:

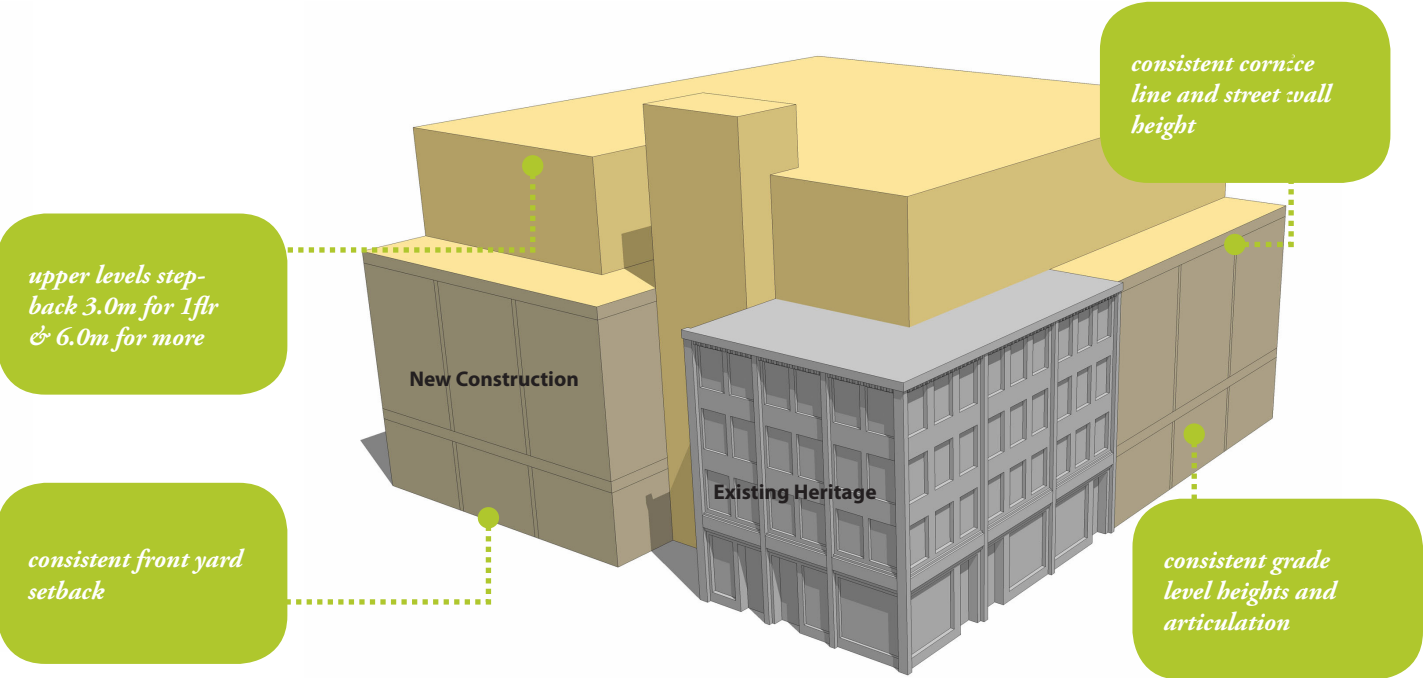
- New exterior facades should define the height and alignment of the former building should complement the retained façade in construction, design and materials
- New exterior facades beyond the former building should be clearly distinguished through setbacks, design or materiality
- New construction above the retained façade should be designed and stepback according to the guidelines contained in **5.3.3 Cornice Line & Upper Level Stepbacks**



Heritage Infill and Additions



Heritage Integrated Development



6.0 implementation strategies

The recommended implementation strategies to put the *Standards & Guidelines* for the 500 Lot Area into place are grouped under two categories - one for regulatory amendments, and the other for supporting programs and projects. These strategies largely emerged in the context of a week-long workshop and stakeholder consultation, and through discussions with City staff. Some are consistent with recommendations made in previous studies and plans.

The regulatory amendments pertain specifically to the City of Charlottetown's Zoning & Development By-Law, and outline the necessary actions to implement the *Standards & Guidelines* from a legal standpoint. These are the basic components of the strategy to implement a new form-based approach to zoning in the 500 Lot Area, and to achieve key objectives identified throughout this project, including to:

- promote heritage preservation;
- establish a transparent and consistent development approval process; and,
- implement predictable bonusing procedures.

Additional implementation strategies are proposed to support the successful integration of the *Standards & Guidelines* into Charlottetown's development context. These recommendations are intended to achieve additional objectives, including to:

- encourage and facilitate a pre-application review process;
- build an understanding and capacity for the new *Standards & Guidelines*;
- recognize and award development and design excellence; and,
- undertake a Waterfront Master Plan.

6.1 Amendments to the Zoning & Development By-Law

The intent of this document is to provide direction with respect to a rational framework accompanied by appropriate development standards and design guidelines. Given the need for clarity and predictability in how development is regulated, assessed and approved, the *Standards & Guidelines* should be adopted within the statutory regulatory framework. Other implementing strategies, including heritage designation, design review procedures and bonusing, are also recommended and should be clearly articulated as part of the regulatory framework for the 500 Lot Area.

To provide a template for further form-based revisions to the Zoning & Development By-Law, it is recommended that a new Section (30) be created that will include standards, policies and procedures for development in the 500 Lot Area. In addition to the new Section, a number of other amendments will be required to implement the *Standards and Guidelines*, including:

- surgical amendments to other sections of the By-Law to ensure consistency in design and regulatory intent (for example in Section 4.60 "Site Development Principles");
- an amendment to Appendix "H" - Zoning Map, to incorporate the new Urban Character Areas/zones;
- an amendment to Appendix "I" - Heritage Area, to expand its boundaries; and,

- three new Appendices:
 - Appendix "J" - 500 Lot Area Map;
 - Appendix "K" - 500 Lot Area Design Guidelines; and,
 - Appendix "L" - 500 Lot Area Height Map.

More detailed guidance for the content and approach to implementing these amendments to the Zoning & Development By-Law are provided below.

6.1.1 Heritage Preservation

Much of the Lot 500 Area is comprised of charming established historical residential neighbourhoods that are integral to the area's attractive character. The heritage policies included in Section 6 of the City's Zoning and Development By-Law provide a number of tools and controls for protecting heritage resources. To be eligible for heritage preservation, properties must first either be designated a heritage property, or be located within the Heritage Area. The enabling legislation for these policies is the Provincial Heritage Places Protection Act, which provides additional tools for heritage protection.

Consistent with past recommendations, the City should amend the map that constitutes Appendix "I" – Heritage Area – of the Zoning and Development By-Law to include all of the lands within the Downtown Neighbourhood and Downtown Mixed Use Neighbourhood Character Areas of the 500 Lot Area (Figure 1). These neighbourhoods are where the vast majority of heritage resources in the 500 Lot Area are located. Expanding the Heritage Area will ensure

that all properties within these key areas are eligible for protection - a key component of the enhanced design review process proposed below for the 500 Lot Area.

This amendment would also be one step towards implementing Section 3.7 of the City's Official Plan, which recommends the existing Heritage Area be expanded to encompass the 500 Lot Area.

As part of its effort to recognize the unique character of the 500 Lot Area, the City should continue designating additional properties inside the Area, but outside the Heritage Area, that qualify as heritage resources.

6.1.2 A New Section 30 - 500 Lot Area

It is recommended that a new section be added to the Zoning & Development By-Law, titled "Section 30 - 500 Lot Area". To avoid duplicating regulatory requirements that are applicable to all development in Charlottetown, while recognizing the unique policies for the 500 Lot Area, it is recommended that the new Section 30 begin with statements that convey the following:

- Notwithstanding the provisions and requirements of the preceding sections of the Zoning and Development By-Law, the following policies and standards shall apply in the 500 Lot Area, as identified on Appendix "J" (500 Lot Area Map).
- "Future development within the 500 Lot Area shall be in conformity with the *Standards* outlined here, and with the 500 Lot Area Design Guidelines as outlined in Appendix "K". Where there is a conflict between

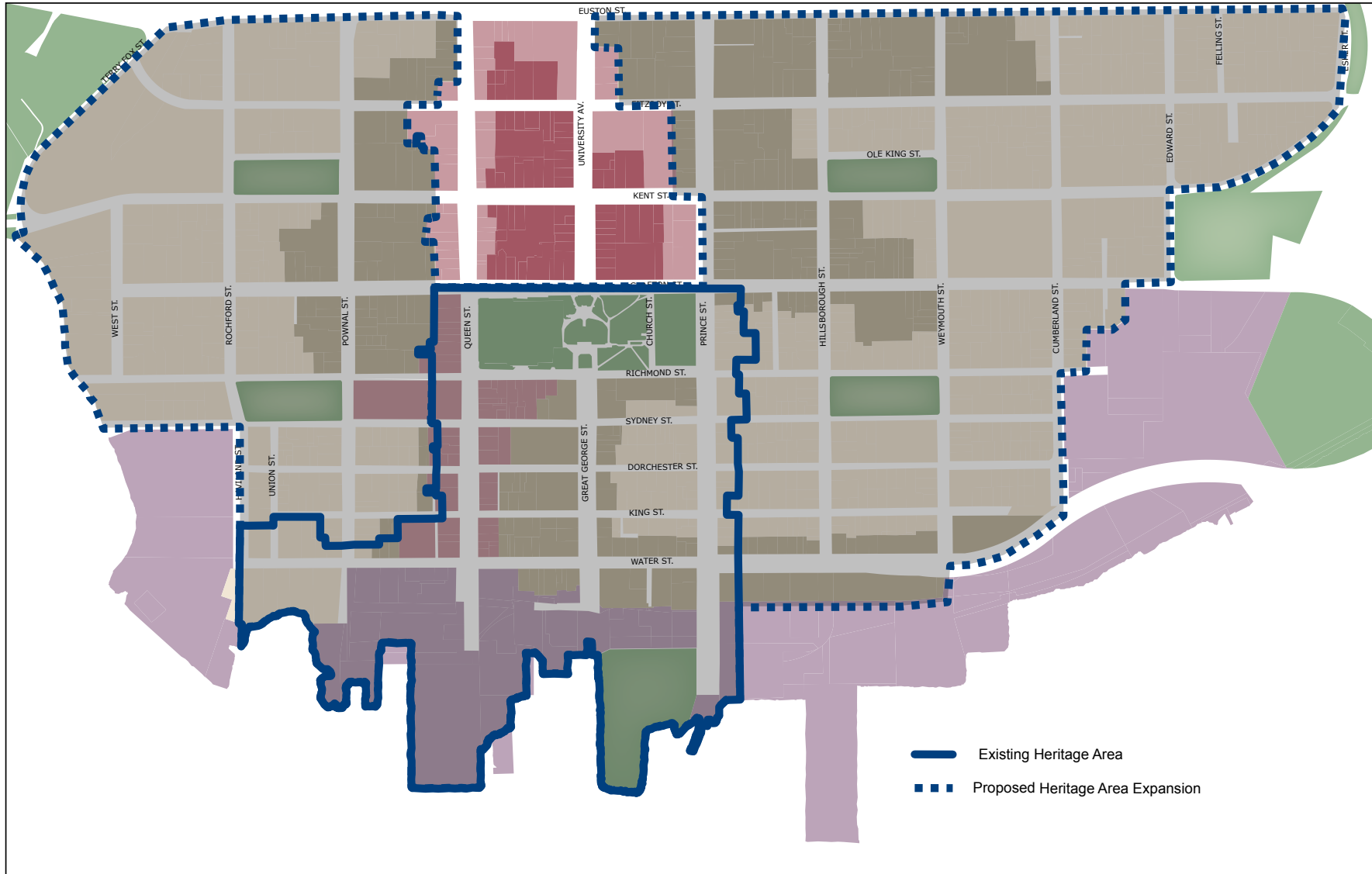


Figure 1. Proposed Heritage Area Expansion

the General Provisions of Section 4 and these *Standards* or the 500 Lot Area *Design Guidelines*, the latter shall apply.”

The remaining content of the new Section 30 for the 500 Lot Area should:

- be consistent with the *Standards* provided in this document, as revised through further consultation;
- provide the *Standards* for each Urban Character Area in an illustrated and user-friendly matrix (for example, see Figure 2); and,
- include provisions for additional information requirements, design review and bonusing, as outlined in the remainder of this section.

> *A New Approach to Design Review & Development Approval*

The need for a new approach to design review was expressed by a broad spectrum of interests, including developers and residents, who expressed dissatisfaction with the current approval process and perceived it to be dysfunctional. On one hand, residents felt that there was not enough transparency and guidance with respect to what was appropriate, on the other, developers felt that the process lacked predictability when it came to decisions of Council. Consequently, the process is perceived as being adversarial with inconsistent outcomes.

As a first step to remedy this situation, the *Standards & Guidelines* are intended to introduce new

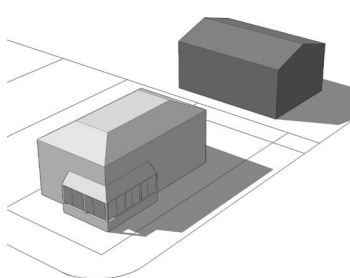
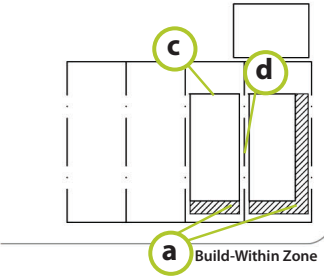
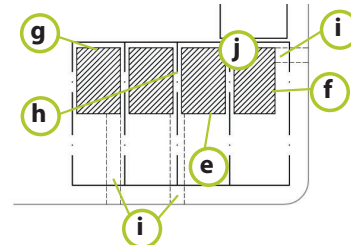
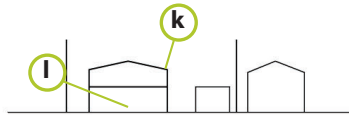
	Uses	Permitted Building Types & Minimum Lot Frontages	Principle Building Placement	Accessory Buildings, Parking & Access	Building Height & Massing																		
Downtown Neighbourhood																							
	<p><i>Permitted:</i></p> <ul style="list-style-type: none"> Park/Open Space Residential Nursing Home Institutional Bed & Breakfast Small-Scale Inns Small-Format Convenience Retail <p><i>Prohibited:</i></p> <ul style="list-style-type: none"> Automobile-Oriented Use/ Drive-thru 	<p>The minimum lot frontage corresponds to building type and shall be consistent with the average of the prevailing lot dimensions of existing properties in the surrounding blocks that contain similar building types. Otherwise, minimum frontages are as follows:</p> <table border="1"> <thead> <tr> <th>Building Type</th> <th>Interior Lot</th> <th>Corner Lot</th> </tr> </thead> <tbody> <tr> <td>Single-Detached</td> <td>10.6m (35ft)</td> <td>12.2m (40ft)</td> </tr> <tr> <td>Semi-Detached</td> <td>7.6m (25ft)</td> <td>10.6m (35ft)</td> </tr> <tr> <td>Duplex/Triple Decker</td> <td>15.2m (50ft)</td> <td>18.3m (60ft)</td> </tr> <tr> <td>Row House/ Stacked Town</td> <td>6.1m (19.7ft)</td> <td>9.0m (29.5ft)</td> </tr> <tr> <td>Walk-up Apartment</td> <td>25.0m (82ft)</td> <td>30.0m (98.4ft)</td> </tr> </tbody> </table>	Building Type	Interior Lot	Corner Lot	Single-Detached	10.6m (35ft)	12.2m (40ft)	Semi-Detached	7.6m (25ft)	10.6m (35ft)	Duplex/Triple Decker	15.2m (50ft)	18.3m (60ft)	Row House/ Stacked Town	6.1m (19.7ft)	9.0m (29.5ft)	Walk-up Apartment	25.0m (82ft)	30.0m (98.4ft)	<p>Principle buildings shall be placed within the shaded area as indicated in the figures and subject to the following standards:</p> <ol style="list-style-type: none"> Front Yard/Flank Yard setback: Any distance within the Build-Within Zone as established by the minimum and maximum front yard setbacks of the prevailing placement of existing buildings on block that share the same street frontage. Otherwise, a minimum 2.4m (8ft) and maximum 4.9m (16ft). Front Yard Encroachments: Elements such as porches, stoops, and bay windows may encroach a maximum of 2.4m (8ft) into the minimum front yard setback subject to maintaining a minimum 1.5m (4.9ft) setback from the street line. Rear Yard setback: The average distance that existing buildings on the block are setback from the rear lot line or a minimum 6.0m (19.7ft), which ever is greater. Side Yard setback: A distance equal to the side yard setback of the existing building on the abutting property, otherwise a minimum of 1.83m (6.0ft). 	<p>Accessory buildings and on-site parking shall be placed within the shaded area as indicated in the figures and subject to the following standards:</p> <ol style="list-style-type: none"> Front Yard setback: The minimum being the distance equal to 50% of the lot. Flank Yard setback: Minimum 6.0m (19.7ft). Rear Yard setback: Minimum 1.2m (3.9ft). Side Yard setback: Minimum 1.83m (6ft) for accessory buildings. Minimum 0.6m (2ft) for parking. Vehicular Access: Permitted through a side driveway. Corner lots are permitted to provide a rear driveway accessed from the side street. Shared and consolidated driveways are encouraged where possible. Car Garage: Rear yard attached or detached garages are permitted. Front yard attached or detached garages that sit proud of the main building wall are prohibited. Side yard attached or detached garages are permitted where the width of the garage frontage is no greater than 50% of the width of principle building's frontage. 	<p>Structures shall comply with the following standards:</p> <ol style="list-style-type: none"> Building Height: Minimum 2-storeys or 7.5m (24.6ft). Maximum 3-storeys or 12.0m (39.4ft). Maximum 1-storey or 7.3m (24ft) for accessory buildings. Grade Level Height: Minimum 4.0 (13ft) floor-to-floor grade level height for residential uses, otherwise minimum 4.5m (14.8ft) for non-residential. Massing: Building profiles, massing and rooflines should be consistent with the character of similar building types on the surrounding blocks and subject to the applicable design guidelines contained in Section 5.0. 
	Building Type	Interior Lot	Corner Lot																				
Single-Detached	10.6m (35ft)	12.2m (40ft)																					
Semi-Detached	7.6m (25ft)	10.6m (35ft)																					
Duplex/Triple Decker	15.2m (50ft)	18.3m (60ft)																					
Row House/ Stacked Town	6.1m (19.7ft)	9.0m (29.5ft)																					
Walk-up Apartment	25.0m (82ft)	30.0m (98.4ft)																					

Figure 2. Example of Standards Matrix

predictability to the development approvals process by providing clear direction on what constitutes appropriate development in a given area. Additional strategies include requiring adequate information for major developments, and revamping the design review and development approval procedures.

Information Requirements for Major Developments

Essential to making an informed decision about how well a development proposal performs with respect to these *Standards & Guidelines*, is adequate information by which to assess appropriateness and impacts. Detailed information will be particularly useful for major development in high priority areas and development that is heritage-related. Such development has the potential to have greater impacts and should be subject to greater scrutiny.

Section 4.60 of the Zoning and Development By-Law already includes information requirements for development proposals in most zones, with the exception of residential properties containing four units or less. Existing information requirements include a site plan, building floor plans, elevations and a written statement with graphics that address how the proposed development is compatible and integrated with existing development.

Additional information requirements should be required for certain development applications within the 500 Lot Area, as follows:

- Additional information requirements should be required for all applications for "significant development" within the 500 Lot Area.

- Significant development shall be defined as:
 - any new building or structure;
 - any addition to, or alteration of a building that results in an increase in Gross Floor Area by 50 percent or more; and,
 - any addition to, or alteration of, an existing Designated, Grade 1 Level, or Grade 2 Level building or structure that results in an increase in Gross Floor Area by 20 percent or more.
- Development proposals that meet the above criteria should be required to provide the following information and studies, as deemed applicable by the Development Officer or Heritage Advisor:
 - A digital or scaled massing model of the proposal in its immediate context;
 - Shadow studies during the shoulder seasons (March 22nd and September 22nd) where buildings over 3-storeys are proposed to ensure no shadow impacts on parks and a duration of no more than 2-hours on adjacent residential properties;
 - Wind studies where buildings over 5-storeys are proposed to ensure comfortable conditions for pedestrians at the sidewalk;

- Heritage Impact Study and Conservation Plan where proposals include a Designated or Grade Level 1 heritage property; and,
- Detailed elevations of the building base (first 3-storeys) at 1:50 scale indicating architectural detailing and materials selection to be secured in a development agreement.

The above triggers and requirements for providing additional information should be outlined in the new Section 30 - 500 Lot Area of the Zoning & Development By-Law.

New Design Review Procedures

In addition to the heightened information requirements, we recommend that the City establish new design review procedures to ensure the professional and transparent review of higher-profile development applications. New design review procedures are needed to supplement the current development approvals process (illustrated in Figure 3), which relies primarily on a Development Officer or Heritage Advisor, and Boards whose membership requirements are not necessarily related to design expertise. The primary intention is to introduce professional expertise from architects and urban designers in the review of major development proposals, specifically to assess the proposals compliance with the *Design Guidelines*.

To ensure the professional and transparent review of major development applications, the City should

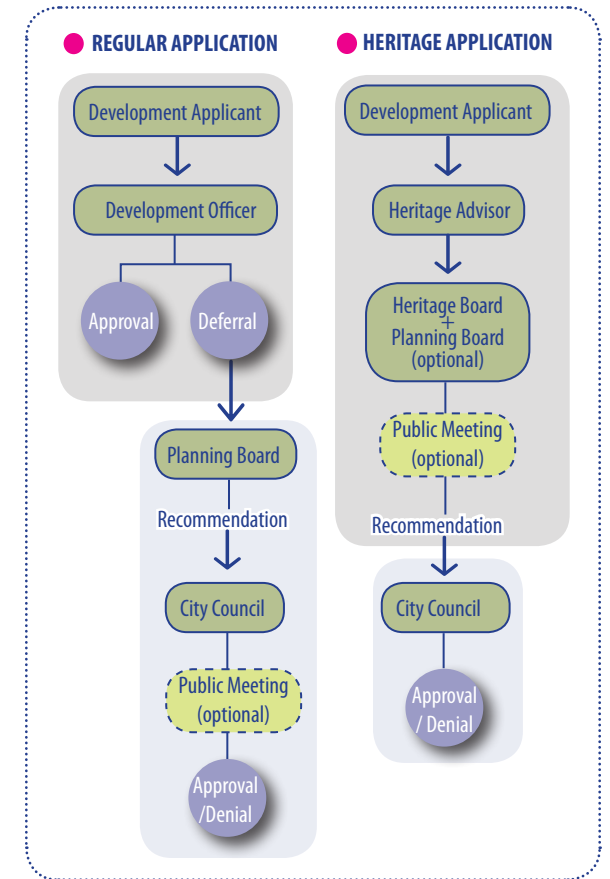


Figure 3. Existing Development Approval Process

include provisions, procedural requirements, and a defined scope for enhanced design review within the new Section 30 - 500 Lot Area of the Zoning & Development By-Law, as follows:

- Enhanced design review should be required for all proposals for "significant development" in the 500 Lot Area.

One or more of the following options for enhanced design review should be selected and outlined. These options may be implemented through a phased introduction:

- *Option A* - Maintain the current approval process, but require the Development Officer or Heritage Advisor (as applicable) to seek the advice of a hired architect when reviewing development applications that trigger enhanced design review. Under this scheme it is recommended that the City hire a qualified architect to specifically fulfill this post.
- *Option B* - Maintain the current approval process, but require the Development Officer or Heritage Advisor (as applicable) to seek the advice of a Design Review Panel (DRP) when reviewing development applications that trigger enhanced design review. Under this scheme it is recommended that a DRP be formally established, and at a minimum include:
 - one planning staff from the municipality;

- three professional experts from the fields of architecture, landscape architecture, urban design, and/or city planning; and,
- one local heritage expert who is a resident of Charlottetown.

- *Option C* - Delegate approval authority so that it is shared between the Development Officer and the Design Review Panel. Rather than serving an advisory role, the DRP would be given authority to evaluate whether a development proposal complies with the *Design Guidelines* and heritage requirements. The Development Officer would continue to assess the proposal's compliance with the *Standards* (Figure 4).
- Prior to enhanced design review by the architect or DRP, all quantitative considerations should be conducted by the Development Officer in accordance with the *Standards*.
- The mandate of the architect or DRP should be to ensure proposals are consistent with the spirit and intent of the *Design Guidelines* for each Urban Character Area (proposed Appendix K of the Zoning & Development By-law).
- In the case of Option C, the Design Review Panel should also be mandated to evaluate heritage elements, in accordance with the *Guidelines* and other existing policies (e.g. the national Design Guidelines for the Preservation of Historic Resources). Under

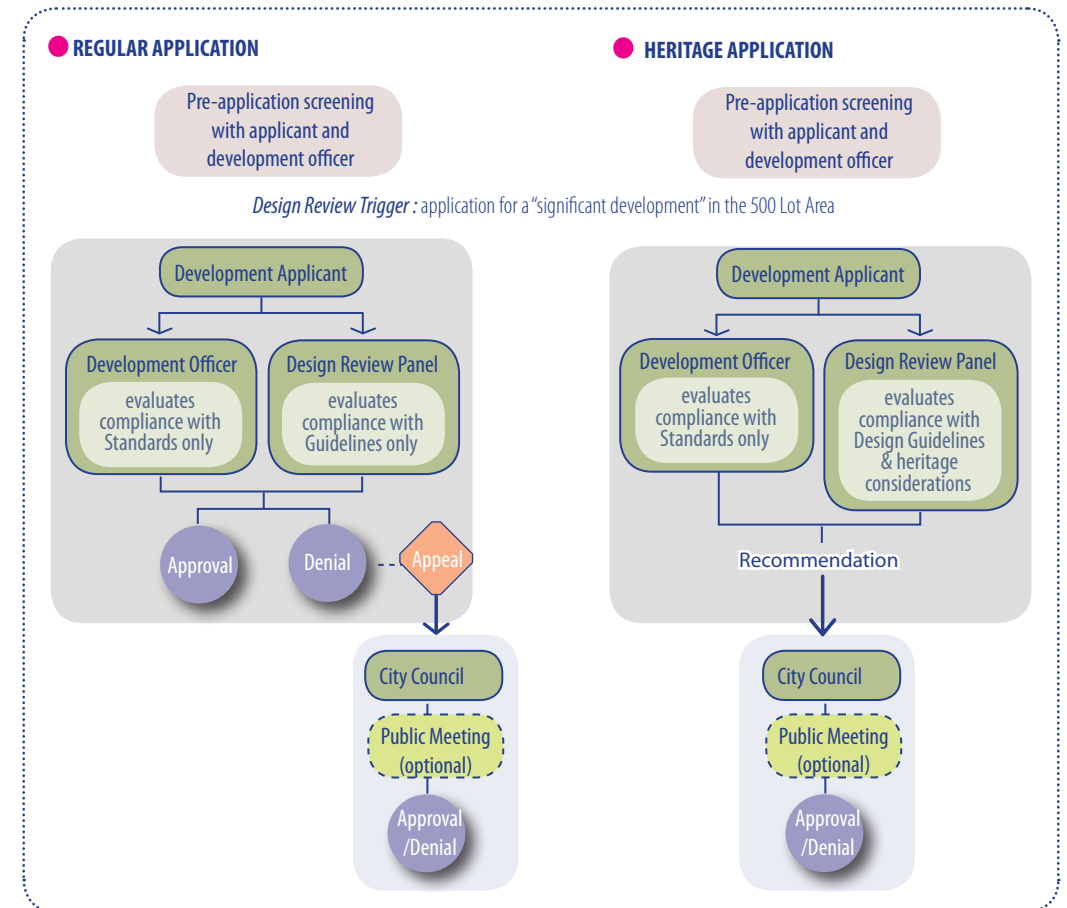


Figure 4. Option C - Proposed Development Approval & Design Review Process

The proposed Option C is the greatest departure from the status quo, but does build upon the existing approvals process, including the existing delegation of approval authority. It seeks to streamline the process while introducing new checks and balances and professional expertise, while maintaining appeal and escalation avenues that introduce a role for Council and the public only in cases of dispute that cannot be negotiated between the applicant and the Development Officer and DRP. For the sake of predictability and fairness, the process is built on the principle that as-of-right developments should only be evaluated against the set of technical Standards and Guidelines outlined in the Zoning and Development By-Law. Applications that require re-zoning, however, should continue to be subject to the existing consultation and amendment process, as outlined in Section 4.27 of the Zoning and Development By-Law.

Options A & B, the Heritage Advisor would maintain this role.

- Any assessment related to heritage resources should include a consideration of the property's heritage rating (Grade 1, 2, 3) and associated guidelines for preservation, as follows:

Grade 1 - the entire interior and exterior of the building should be preserved;

Grade 2 - exterior restoration and maintenance is encouraged, and the entire building should be preserved; and,

Grade 3 - suitable exterior alterations and modifications are permitted (Designation, if applicable, is intended to prevent unwarranted demolition or insensitive alteration).

- A set time limit (e.g. 60 days) should be specified for the development approval process, including the design review, to provide an additional level of predictability.

It should be noted that (in addition to enhanced design review and information requirements) all development applicants would still be subject to the basic requirements outlined under Section 4.60 of the Zoning & Development By-Law.

> *Bonus Height*

Despite the development standards of the in-force Zoning & Development By-Law, developments have occurred that often far exceed permissions. Consequently, variances for taller buildings are

occurring without clear guidance with respect to appropriate design and performance standards. The *Standards & Guidelines* outlined in this document accept that taller buildings already exist and propose appropriate development parameters to consistently guide such proposals that may come forward in the future. Furthermore, consistent with the guiding principle that larger developments have the greatest civic obligations, the *Standards* require that all developments that exceed the base 3-storey height limit do so only as a bonus in exchange for defined community benefits.

While the *Development Standards* clearly set out the appropriate building envelopes, clear procedures for attaining the bonus permissions are also required to ensure a credible and predictable process. The policies proposed below are intended to be included in the new Section 30 of the Zoning & Development By-Law, and are based on the provision of community benefits in exchange for bonus height - heritage preservation being the first priority:

- Bonus height may be granted to development within the 500 Lot Area in the following character areas:
 - Downtown Main Street
 - Downtown Core
 - Downtown Waterfront
 - Downtown Mixed Use Neighbourhood

- Bonusing and/or variances in height shall not be permitted in the Downtown Neighbourhood character area.
- A new 500 Lot Area Maximum Height Map should be provided as a new Appendix "L" (Figure 5).
- An increase in height above the as-of-right height indicated in the *Standards* and on Appendix "L" may be provided in exchange for community benefits, as defined in this section. The increase in height shall not exceed the height shown on Appendix "L", and no further variance in height in excess of the maximum shown on Appendix "L" shall be permitted.
- Community benefits may include:
 - the preservation of a heritage building in its entirety on the property or another property within the 500 Lot Area;
 - the preservation of façades of a heritage building on the property or another property within the 500 Lot Area;
 - the provision of a public cultural amenity space (such as a gallery or museum) of no less than 140.0sm (1,500sf);
 - the provision of affordable housing units;
 - a publicly accessible open space or mid-block connection where appropriate and desirable;

- a public art installation on the site or elsewhere in the 500 Lot Area;
- City defined public realm improvements (such as streetscaping or park improvements) on site or elsewhere in the 500 Lot Area;
- the provision of amenities that support active transportation, including but not limited to bicycle parking for visitors and residents/employees, benches, and transit shelters; and,
- building design that achieves sustainability measures outlined in the City of Charlottetown's Sustainable Green Development Standards.
- In the case of ownership housing, affordable housing is defined as the least of:
 - housing for which the purchase price results in annual accommodation costs which do not exceed 30 percent of gross annual household income of those households below the median income of the Charlottetown Census Subdivision; or
 - housing for which the purchase price is at least 10 percent below the average purchase price of a resale unit in the regional market area.
- In the case of rental housing, affordable housing is defined as the least expensive of:

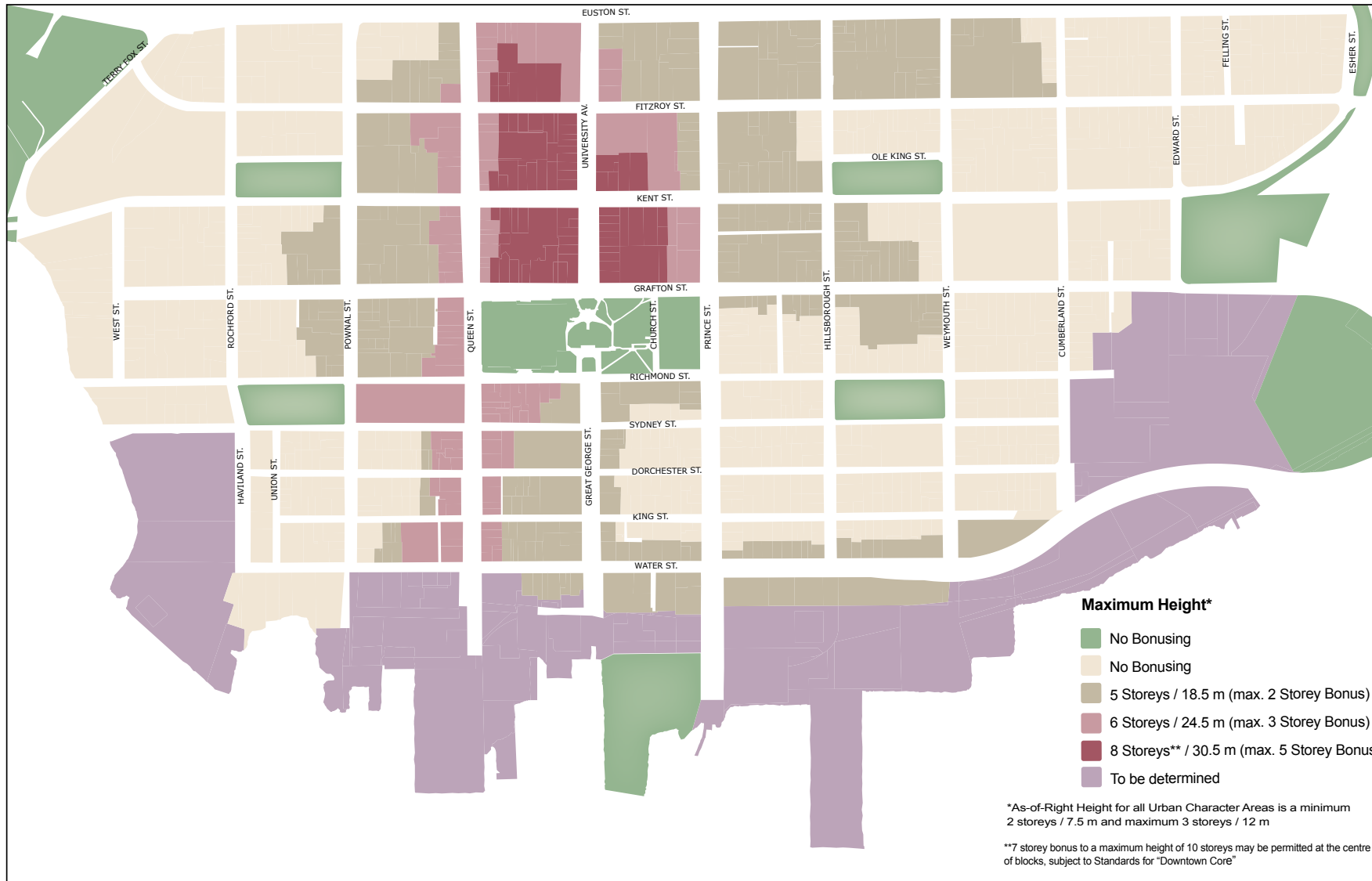


Figure 5. Proposed Appendix "L" - 500 Lot Area Maximum Height Map

- a unit for which the rent does not exceed 30 percent of gross annual household income of those households below the median income of the Charlottetown Census Subdivision; or
 - a unit for which the rent is at or below the average market rent of a unit in the regional market area.
- Where a heritage property is located on site, heritage preservation shall be prioritized above all other types of community benefits.
 - As an alternative to physically providing the defined community benefit(s), applicants may provide cash-in-lieu at the rate of \$10 to \$20 per square metre, in exchange for an increase in height up to the maximum shown on Appendix L. The cash-in-lieu of community benefits shall be retained by the City in a "community benefits bank", and be used to provide defined community benefits within the 500 Lot Area.
 - The community benefit, or cash-in-lieu, provided in exchange for bonus height shall be negotiated on a site-by-site basis between the applicant and the Development Officer, who shall consider the guidance of the Design Review Panel, if it is active. The bonus height shall be secured through a legal agreement (the Bonus Agreement) to be registered on title.

6.1.3 Complementary Amendments

> Incorporate the Urban Character Areas

To give effect to the new Urban Character Areas (the *Standards* for which will be provided in the new Section 30), they will need to be incorporated into the Zoning Map, and included in select sections of the By-Law to specify whether or not general policies apply. As such, it is recommended that Council:

- Amend the Zoning Map (Appendix “H” of the Zoning and Development By-Law) to delete all existing zone designations within the 500 Lot Area, and replace those with the Urban Character Areas as shown on **page 10** of this document and as defined in the *Standards*. The Urban Character Areas shall have the regulatory effect of zones, and include:
 - Downtown Neighbourhood (DN),
 - Downtown Mixed Use Neighbourhood (DNMU),
 - Downtown Main Street (DMS),
 - Downtown Core (DC),
 - Downtown Waterfront (DW).
- Amend Section 8 “Zones” to include the Urban Character Areas/zones.
- Amend items 1 and 2 of Section 4.60 Site Development Principles of the By-Law to include all the new Urban Character Areas in the list of zones subject to site plan review.

- Identify and amend any other General Provisions in Section 4 of the By-Law to include the appropriate Urban Character Areas in any relevant list of zones to which a provision applies.

> Bonusing

To give effect to the bonusing policies Council will need to undertake the following additional actions:

- Amend Section 4.28 Minor Variances and 4.29 Other Variances to specify that variances for building height shall not be permitted in the 500 Lot Area. Increases in height in the 500 Lot Area may be permitted under the bonusing provisions of Section 30.

> Delineate the 500 Lot Area

To provide a visual reference for the area to which the 500 Lot Area policies, procedures, *Standards & Guidelines* apply, it is recommended that Council:

- Insert a new Map to delineate the 500 Lot Area. This new Map should be inserted as Appendix “J” and titled “500 Lot Area”.

> Adopt the Design Guidelines

As part of implementing an enhanced design review process, Council should:

- Insert the *Design Guidelines* as Appendix “K” to the By-Law, titled as “500 Lot Area Design Guidelines”.

6.2 Supporting Policies & Programs

To successfully integrate the *Standards & Guidelines* into Charlottetown’s development context, a number of supporting policies and programs are proposed, including to:

- encourage and facilitate a pre-application process;
- build an understanding and capacity for the new *Standards & Guidelines*;
- recognize and award development and design excellence; and,
- undertake a Waterfront Master Plan.

> Encourage and Facilitate a Pre-Application Consultation Process

A number of regulatory amendments have been proposed for improving transparency in the decision-making process. One additional strategy for preempting unnecessary controversies is to encourage and facilitate a pre-application consultation process, which already occurs in a less formalized manner.

A pre-application process has been proven to be successful in other jurisdictions. Particularly where significant developments are proposed, support is much more likely to be gained when the community is consulted early in the design development process. Proponents can better gauge the expectations and desires of interested parties, while the community gains a sense of ownership in the process as well as an appreciation of potential

challenges faced by the developer. Not only does this approach lend to a more transparent process, it can also serve to mitigate potential unanticipated conflicts.

To encourage and facilitate a pre-application consultation process, the Council should direct staff to:

- take an active role in organizing the meetings and notifying stakeholders;
- promote the benefits of the pre-application process among planning and heritage staff; and,
- develop and publish material explaining that the opportunity for pre-application consultation exists.

Informational material should be made available on the City’s web site, on the Planning & Heritage page, currently found at: <http://www.city.charlottetown.pe.ca/planningandheritage.php>.

> Build an Understanding & Capacity for the new Standards & Guidelines

These *Standards & Guidelines* introduce a significant departure to the status quo and may require broad support and political will to implement. To do what is being proposed, the underlying principles and rationale will need to be properly communicated to build an understanding amongst all stakeholders as well as Council. Capacity-building with City Staff will also be required to ensure its proper implementation and application. To ensure that

this document doesn't fall victim to inaction as with previous studies, it is recommended that prior to completing any further revisions and/or gaining approval, a series of stakeholder workshops be held to present and translate the *Standards & Guidelines*.

At a minimum, these stakeholder workshops (with residents, developers, landowners, staff and Council) could consist of a highly graphic and accessible presentation of the draft *Standards & Guidelines* followed by an interactive exercise designed to solicit constructive feedback about what is supported and what may need further clarification or exploration.

These workshops should form part of the consultation process leading up to the adoption of the 500 Lot Area Development Standards and Design Guidelines (through a comprehensive amendment to the City's Zoning and Development By-Law, as proposed in Section 6.1 of this document). Specifically, these workshops could be organized to lead up to a final statutory public meeting, as required under Section 4.27 of the By-Law – Amendments to the Zoning and Development By-Law.

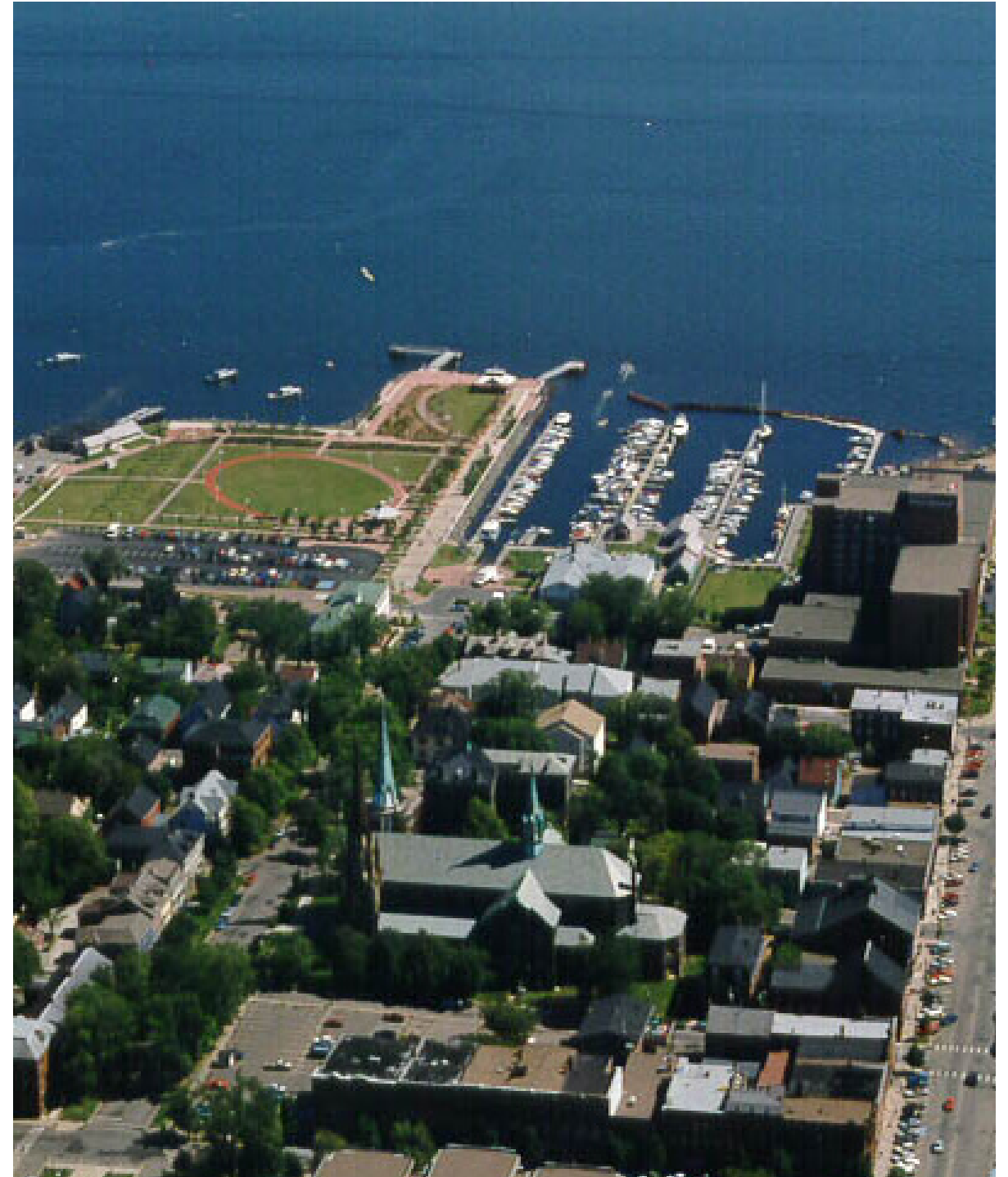
> *Recognize & Award Development & Design Excellence*

Consistent with the guiding principle that encourages the designing of buildings that will become the future heritage in the 500 Lot Area, the City should establish an annual Urban Design Awards program to recognize not just historic preservation, but all worthy design excellence in

city-building – architecture, landscape architecture, small and large scale developments, public art, etc. This recognition can serve as an incentive for good design and can help to further nurture a design-oriented development culture by fostering a healthy competitiveness amongst designers and builders.

> *Undertake a Downtown & Waterfront Master Plan*

Stakeholders consistently identified the need for a guiding vision for the Waterfront, as well as the Downtown as a whole. Although, these *Standards & Guidelines* provide direction with respect to built form and provide a high-level urban design framework, far more would be needed to complete the vision 'picture'. Especially needed are a vision and coordinating plan to guide the enhancement of, and investment in, the public realm. Furthermore, a master planning process can be an important community defining and building exercise, where a broad spectrum of interests can come together to generate a commonly shared vision and the process, momentum for change can be inspired.



appendix: stakeholder meetings

The Consultant Team met with the following individuals as part of the stakeholder interviews undertaken during the workshop held between September 20th and 24th 2010:

Residents

Andrea Battison (Downtown Residents Group)

Joan Cumming (Resident & Business Operator)

Malcolm Lodge

Anita Mercier

Edward Rice

Developers

John Horrelt

Rob McCluskey

Michael Murphy

Kevin Murphy

Paul Smith

Architects

Bill Chandler

David Lopez

Other Organizations

Dawn Allen - Downtown Charlottetown Inc.

Peter Hyndman - Mayors Ad Hoc committee & Chamber of Commerce

Ernie Morello - Charlottetown Area Development Corporation

City

Kim Devine – Chair & City Councillor on the Planning Committee

Catherine Hennessey - Heritage Advocate and Planning friend

Todd Saunders - Heritage Advisor

Don Poole - Manager of Planning Department

The following is a summary of the key themes raised in the stakeholder meetings:

- The approval process is dysfunctional where Variances and Heritage is concerned – this is polarizing the community
- Developers invest a great deal of money, time and effort with the Planning Board and Staff only to get turned down by Council
- Perception that Councillors are not informed enough about the proposals and the process and therefore, no consistent decision-making
- The Heritage Board needs more guidance to avoid arbitrary decisions
- Council needs to be educated on the value of the downtown and heritage
- Developers and residents want predictability, clear rules and regulations
- Residents feel that there is no protection for the neighbourhoods and that heritage is not valued enough
- Some developers have experienced great success and profitability with respect to heritage
- Residents are concerned about views and access to the waterfront
- Perception that the Planning Board simply rubber stamps all development proposals
- Heritage expertise should be a resource to developers not a watchdog - Heritage Board is feared rather than embraced as constructive to the process
- There is not a single unified vision for the Downtown of the future
- The City needs to pick and understand its market and be the best in the world at it